

City of Healdsburg

Design Review Manual



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Introduction

Healdsburg's natural scenic beauty and its unique built environment define the quality of the city and constitute its visual image. These resources provide a framework within which to address scenic and urban design issues and upon which to build a more coherent and attractive city. The City of Healdsburg has made a commitment to enhance its image through appropriate development, good design and preservation of specific scenic resources. In accordance with this commitment, a seven-member board, the Planning Commission, has been appointed by the City Council to assure that new developments are both well designed and consistent with the City's General Plan and other established ordinances and policies.

It is the purpose of this manual to establish the guidelines to be used by the Planning Commission in evaluating design proposals. It is intended that these standards will assist in providing adequate direction for the project owner or professional designer in establishing parameters for their projects. The following general policies are the underlying basis of the specific guidelines and criteria contained within:

Design Review Policies

1. It is the duty and function of the Planning Commission to preserve natural beauty, promote attractive developments of land and, where possible, improve the appearance of the community.
2. The Planning Commission's concern shall be the site and architectural design of any land use and structures permitted by the building and zoning regulations insofar as such features affect the public interest and have impact on the use and enjoyment of other properties. The Planning Commission shall also review any change in use on an existing, developed property.
3. In granting approvals, the Planning Commission shall attempt to prevent the indiscriminate clearing of property, the unnecessary destruction of trees and shrubs and the gross alteration of the natural topography and drainage.
4. The Planning Commission will promote projects that will stimulate new uses and structures that are compatible with the standards set herein.
5. Signs which are unsightly or obnoxious in appearance, which are not properly related to the site or use, or not harmonious with adjacent uses shall be prohibited.
6. The Planning Commission shall attempt to obtain the best possible traffic circulation in the project area.
7. Features which are ugly, inharmonious to the site or neighborhood, or monotonous shall be prohibited. Originality in site design, architectural design and landscape design shall be encouraged.

On the basis of these general policies and specifics, it is hoped that the design review process can achieve a balance between the project needs and the broader community needs.

Section 1 General

While encouraging the broadest possible range of individual and creative design, the following standards shall apply to all development.

1. Preservation of Natural Site Amenities

Creeks, views, mature trees and tree groupings, significant vegetation masses, and significant topography should be preserved and incorporated into development proposals. Such features should be considered as strong site design determinants.

2. Relationship to Existing Development

New development is not necessarily required to match surrounding, existing development. However, in areas with a distinct character, compatibility with such design elements as setbacks, massing, materials, colors, roof pitch and fenestration is encouraged. The siting and orientation of buildings should respond to the pedestrian or vehicular nature of the abutting street(s). Buildings on streets of high pedestrian use should face onto and be directly accessible from the sidewalk. Buildings on streets with heavy vehicular traffic, particularly those with no on-street parking, should provide a major entry from the off-street parking area. However, the building should address the street architecturally.

3. Site Relationships

Projects containing many buildings should generally provide variety in building size and massing. A transition from low buildings on street frontages to larger and taller structures on the interior of the property is generally encouraged.

On larger sites, a portion of the total building area should be located at the street perimeter to reinforce and strengthen the streetscape, and to help screen off-street parking areas.

Multi-tenant buildings, centers and complexes should have a unified design theme, including the consistent use of architectural elements, materials, colors and textures. Additions and alteration must be consistent with and enhance the design theme. Individual storefronts may vary to reflect the character of the use within, but the overall theme shall be respected. All building elevations should be architecturally treated in a consistent manner, including the incorporation within the side and rear building elevations of some or all of the design elements used for the primary facades, and the consistent use of colors and materials. Parking structures should also incorporate some or all of the design elements used for the primary facades.

Section 2 Architecture

Building design should create visual character and interest by using shapes, colors, materials and other architectural treatments to define, differentiate, soften and enliven the built form. The design of exterior surfaces should produce effects of texture and relief that provide character and reduce the overall scale of a building.

1. Building Colors

Building colors should be compatible with the surrounding area and should reinforce the visual character of the environment of the proposed buildings

2. Building Materials

Building materials and their use should respect the character of the City, and the context of the individual site.

3. Entryways

Main entryways to buildings should be clearly defined and smoothly integrated with building and landscape designs. They should be architecturally detailed to serve as focal points and pleasant invitations to visitors. Pleasing effects can be achieved by canopies, roof overhangs, recessed areas and frame line extensions.

4. Windows and Doors

Windows and doors should be chosen and located to enhance the architecture, provide continuity with forms, and create patterns of relief in facades. Openings should not be located simply according to interior requirements, but should be treated as part of an architectural composition as well.

The variety, scale and rhythm provided by window and door openings can help to establish character and variety, especially for large, otherwise flat and uninterrupted expanses of exterior walls. Recessed openings can be used to provide contrast through varying patterns of shade and sunlight, and depth of flat planes.

5. Roofs

When exposed to view, roofs are a significant factor in establishing a building's appearance and character. Roofs that are visible should be an attractive part of a building's total design and respond harmoniously to nearby or adjoining structures.

6. Utilities and Mechanical Equipment

Mechanical equipment and accessory utility facilities shall be architecturally screened from view. Such features should be either enclosed by outer building walls or parapets, or grouped and screened in a manner that integrates them with the design of the building. Minor features not exceeding one foot in height shall be exempted from this regulation. Rooftop mechanical equipment and appurtenances to be used in the operation or maintenance of a building should be arranged so as not to be visible. This requirement shall apply to the construction of new buildings, and to any alteration of mechanical systems of existing buildings that results in significant changes in such roof-top equipment and appurtenances.

7. Utilitarian Design Elements

Utility doors, fire doors, loading docks and other potentially unsightly elements should be concealed from view or designed to blend with the building's architecture. Downspouts should be painted to match the wall color or be concealed within the walls, unless they are used as an architectural detail.

8. Trash Enclosures

Trash areas shall be concealed from public view and should be surrounded by a five-foot high, opaque enclosure with solid gates, of a design compatible with that of the main buildings, and using a similar palette of building materials and colors.

9. Exterior Lighting

Exterior lighting shall be stationary and deflected away from adjacent properties. The light source shall not be visible off site. Light fixtures should be limited to a height of 14 feet and the fixture style and design should be compatible and consistent with the building design.

Section 3 Parking

1. Purpose

These regulations are intended to provide accessible and attractive off-street parking facilities in the city's commercial, industrial and multi-family projects.

2. Applicability

These standards shall apply to all projects where multiple vehicle accommodation is required.

3. Dimensional Requirements

A. Minimum Dimensions

Required minimum parking dimensions for standard parking spaces are indicated in the following table and depicted on Diagram A, "Minimum Parking Dimensions".

Parking Angle	Width (W)	Depth (D)	Length (L)	1-Way Drive (A)	2-Way Drive (A)
parallel	9'	--	22'	18.5'	20'
30°	9'	16'	18'	12'	20'
45°	9'	19'	18'	13'	20'
60°	9'	20'	18'	18'	20'
90°	9'	18'	18'	24'	26'

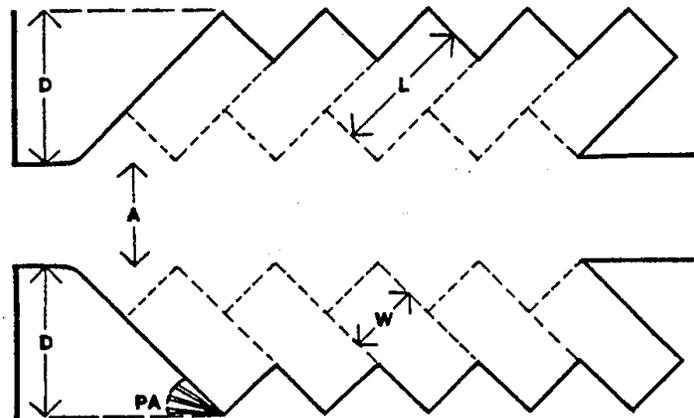


Diagram A - Minimum Parking Dimensions

B. Spaces Adjacent to Walls

Any parking space located parallel to a wall or other solid barrier shall be widened an additional two feet.

C. Compact Spaces

Up to 25% of the total parking space for a project having at least 12 parking spaces may have minimum dimensions of 16 feet in length and 8 feet in width where such parking is located adjacent to landscaping.

4. Landscaping

The following landscaping requirements are minimum standards, all of which must be met. Exceeding these minimum requirements is encouraged.

- A. At least 12% of a vehicle accommodation area shall be landscaped. The vehicle accommodation area means the area of a site used by vehicles for access, circulation, parking, and loading and unloading activities; it does not include space provided for display purposes or enclosed vehicle storage areas.
- B. A landscape planter with a minimum width of five feet shall be provided adjacent to any public or private street wherever parking or circulation is generally located adjacent to such rights-of-way. The planter area shall be credited towards the minimum 12% landscaped area required for the site.
- C. A minimum of one tree shall be provided for every 10 parking spaces not located on the perimeter of the parking area and shall be located so as to visually disrupt long rows of parked vehicles (trees may be clustered). Canopy-type trees should be used to provide a relatively consistent tree cover that will shade the pavement and vehicles. Trees should also be provided at appropriate intervals between perimeter parking spaces.
- D. Landscaped areas shall be distributed throughout the entire vehicle accommodation area as evenly as possible and as required by the City Planner or Planning Commission. In larger parking areas (two or more maneuvering aisles), interior landscaping shall be provided to additionally screen parking areas and to visually separate the parking areas into small spaces.
- E. Where vehicles are to be parked immediately adjacent to a public or private street, an opaque screen with a minimum height of 30 inches, measured from the finished surface of the parking area, shall be provided
- F. All landscaping areas within or adjacent to paved areas shall be protected by a curb not less than six inches high or by concrete bumpers where parking spaces abut or overhang landscaping.

5. Access

- A. Any parking area shall be provided with suitable maneuvering room so that all vehicles therein may enter an abutting street in a forward direction.
- B. Use of a required parking space shall not require more than two vehicle maneuvers, except for parallel parking spaces.
- C. No parking space shall be located so that a vehicle is required to maneuver within 10 feet of a vehicular entrance to the property.
- D. No required parking space shall be located so as to require the moving of another vehicle in order to access the space.
- E. Access to one or more automobile parking spaces shall be not less than 10 feet wide at any place.

6. On-Site Circulation

- A. Vehicle accommodation areas shall be designed so that emergency and other public service vehicles can provide service without backing unreasonable distances or making other dangerous or hazardous turning movements.
- B. Separate vehicular and pedestrian circulation systems should be provided within a parking area where possible. Pedestrian linkages between uses in commercial developments should be emphasized, including distinct pedestrian access from parking areas in large commercial developments.

7. Surfacing

All parking and loading areas shall be surfaced with a minimum thickness of two inches of asphalt concrete (Type A) over a minimum thickness of six inches of Class II aggregate base material or a minimum thickness of six inches of Portland Cement Concrete. Such surfacing shall be permanently maintained free of structural defects.

8. Marking

- A. All parking spaces shall be striped in a manner clearly showing the layout of the intended parking stalls. Such striping shall be maintained in a clear and visible manner.
- B. Striping between parking spaces shall take the form of two parallel, four-inch wide stripes, separated by 18 inches and connected with a curve at the bottom.
- C. All pavement markings shall be white, with the exception of handicapped parking markings.

9. Lighting

- A. Parking areas designed to accommodate three or more vehicles in other than vehicle sales areas shall have lighting facilities capable of providing sufficient illumination at every point of the parking area.
- B. Any illumination, including security lighting, shall be so arranged as to reflect away from adjoining properties and rights-of-way.

10. Slope

- A. Parking areas used exclusively for parking and vehicle maneuvering shall be designed and improved with grades not to exceed a 5% slope.
- B. Driveways used exclusively for ingress and egress or interior parking lot circulation shall be designed and improved with grades not to exceed 20% slope.

11. Drainage

All off-street parking and loading facilities shall be so designed that surface water will not drain over any sidewalk.

12. Screening Adjacent To Residential Zones

Parking areas for three or more cars abutting residentially-zoned property shall be separated from such property by a landscaped strip with a minimum width of five feet, and a six-foot high solid fence or wall, located on the residential side of the landscaped strip.

13. Sight Distance

Walls, dense landscaping and other opaque screens within parking areas shall have a maximum height of 30 inches to allow for maximum visibility.

14. Bicycle Parking

Bicycle parking should be provided in parking lots. Such parking should be located in highly visible locations and should be lockable.

Section 4 Landscape Design

1. Purpose

These design standards are intended to enhance the appearance of development within the City by providing and maintaining landscaping for aesthetic and functional purposes.

2. General Design Standards

- A. Planting materials shall be of a type, size and placement compatible with the project and surrounding land uses.
- B. The scale and nature of landscape materials should be appropriate to the site and/or structure. Large-scale buildings generally should be complemented by large-scale landscaping. Street trees shall be provided as recommended in the General Plan.
- C. Landscaping shall be used to separate parking areas from buildings, and to provide a visual landscaped foreground for buildings.
- D. Landscape plans for open space areas containing significant natural vegetation shall preserve existing vegetation to the greatest extent feasible and integrate it with new landscaping.
- E. Where feasible and physically possible, a minimum of 10% of a project's net site area in a commercial or industrial zone shall be landscaped with lawn or groundcover, shrubbery, flowers and trees. Incidental walkways, if needed, may be developed in the landscaped area.
- F. All required landscaping and planting areas shall have a minimum dimension of three feet in any direction. Peripheral landscaping generally should not be less than five feet in width.
- G. The use of native plant materials and drought tolerant plant materials and the use of drip irrigation are encouraged in order to conserve water.
- H. Gravel, colored rock, bark and similar materials are not acceptable as ground cover. Bark should only be utilized as mulch, not as a permanent form of ground cover.
- I. All landscaping within paved areas shall be contained within a brick or masonry planter box or concrete curb not less than six inches high.
- J. Slopes shall be planted with hardy, well-adapted and drought resistant plants. Rooted cuttings of permanent ground cover plants shall be planted or the slopes shall be hydroseeded.
- K. The use of large expanses of lawn is generally discouraged unless planned in conjunction with areas to be used for outdoor activities or occasional foot traffic. Groundcovers providing textural variety and seasonal accent are preferable in limited areas.
- L. Outdoor activity areas such as swimming pools, sport courts and play areas shall be harmonious with the buildings, the property and the neighborhood. They should be located to allow for visual supervision with minimum impact on surrounding areas from noise and lighting.
- M. Landscape elements such as fountains, sculpture, decorative benches, decorative planters, etc. are encouraged
- N. All landscaped areas shall be served by an automatic irrigation system.

3. Street Trees

- A. The arrangement of street trees shall be compatible with existing development both in species selection and spacing on individual streets.
- B. On new streets, street trees shall be located to create an edge to the road.

- C. Street trees shall be selected which create an intimate scale along the streetscape and do not overpower narrower streets.
- D. Street trees of a larger scale are preferable for wide streets and boulevards, and should be selected to provide effective shading of the street.
- E. Street tree selection shall comply with the recommendation of the General Plan and in the case of the Downtown Area with the recommendation of the Streetscape Plan.

Section 5 Heritage Tree Preservation

1. Where heritage trees are located on a site proposed for development, the requirements of the City's Heritage Tree Ordinance shall be adhered to.
2. The initial project layout, design and grading for the site shall be directed toward preservation and maintenance of these trees.
3. The proposed grading and improvements plan shall reflect consideration of the following safeguards as necessary for the particular species:
 - A. Location in minimum growing areas as required by the species.
 - B. No disruption or removal of structural or feeder roots.
 - C. Protective fencing.
 - D. No filling, development or compaction of the soil surface within drip lines, or only to the limited degree that can be tolerated by the species.
 - E. Such other measures required by the species of tree to be preserved and as recommended by the arborist or landscape architect in the tree report.
 - F. A tree report prepared by a certified arborist will be required. This report shall describe measures necessary to protect the tree and promote continued health. The measures listed in the tree report shall be reflected in the submittal. Proper implementation of protection measures will be a requirement of the project.

Section 6 Fences and Walls

The following design standards shall apply to fences and walls on commercially- or industrially-zoned property:

- A. All sides of perimeter fencing exposed to public view shall be finished in a manner compatible with a project's materials, finishes and colors.
- B. All fences and walls, excluding masonry and approved permanent-finish panels, shall be painted in a consistent and non-garish color scheme that blends with the surrounding environment. All proposed unpainted wood surfaces should be treated or stained to preserve and enhance its natural color.
- C. No portion of a wall or fence shall be used for advertising or display. No barbed wire, concertina wire or grape stakes shall be permitted as fencing material if the fence is visible from the public right-of-way.

Section 7 Outdoor Storage and Service Areas

Outdoor storage and service areas on land zoned commercial, industrial or public shall be subject to the following design standards:

- A. Storage areas shall be limited to the rear of a site, and shall not be located nearer than 50 feet to the front property line, when not screened from view.
- B. Any outdoor area used for storage shall be completely enclosed by a solid wall constructed of concrete, wood, stone, brick or slumpstone with a solid gate, not less than eight feet nor more than 15 feet in height. Chain link fencing with slats or vine covering will only be considered in rare circumstances in areas of low visibility.
- C. All portions of door storage and display areas shall have adequate grading and drainage and shall be continuously maintained.
- D. All outdoor storage areas shall be screened, when possible, with landscaping.

Section 8 Signs

1. General Design Standards

All signs shall be designed in accordance with the following standards:

- A. Signs shall primarily identify the business, establishment or type of activity conducted on the same premises. No sign advertising something that is not the principal business of the site shall be allowed.
- B. Signs shall be restrained in size and design, and as an identification device shall not excessively compete for the public's attention.
- C. A sign's materials, colors, texture, size, shape, height, placement and design shall be harmonious with the building, property and neighborhood of which it is a part. Components not bearing a message (such as supporting structures and backs) shall be constructed of materials and colors that blend with or compliment the surroundings.
- D. Sign size shall be consistent with the requirements of the Zoning Ordinance.
- E. Vending machines when visibly displayed on the site shall be considered signage and added to the total allowable. The placement and illumination of vending machines shall be subject to Design Review approval.
- F. Wall signs shall not be permitted to cover distinctive architectural details.
- G. The base of a monument sign shall be set back at least 5 feet from any public right-of-way and shall be located in a planter area. Vehicle sight distance shall be maintained in accordance with the vision clearance requirements of the Zoning Ordinance
- H. Monument signs shall be provided in lieu of pole signs for all projects. The maximum height of a monument sign is 8 feet as measured from grade or as measured from the base of the modified grade if the sign is proposed to be located on a berm.

2. Sign programs for Multi-Tenant Buildings

A sign program will be required for multi-tenant buildings. Such program shall include exhibits showing the size, color materials, lighting and orientation of all proposed signs and existing signs proposed to remain. Sign permits shall be issued only for tenant signage that is in conformance with the Planning Commission approved sign program.

Section 9 Residential Development

1. Multi-Family Residential Development

The following design standards shall be incorporated in and provided for in all multi-family projects:

- A. Projects shall be designed to maximize common space. When possible all units should be provided with private, secure outdoor space. Units should be arranged to provide maximum visual control from units to common areas. Entryways should be visible from other units or by drive-by police.
- B. Building design and orientation shall recognize the existing streetscape and proposals should attempt to incorporate appropriate elements which cause the design to blend with the surrounding community. The use of front porches, walkways and traditional front yard landscape is encouraged in existing residential neighborhoods.
- C. Accessory buildings and uses (e.g., garages, bicycle storage, laundry rooms, car washing areas, recreation facilities, etc.) shall utilize a design similar to the project's dwelling units, including materials and colors.
- D. Parking facilities should not be located on the street side of the project, nor should garage doors or carports be oriented to the street frontage.
- E. Address numbers shall be uniformly located throughout a project and clearly visible from the street. An address location map shall be provided at the entrance to a project containing more than 20 dwelling units, unless otherwise specified by the Fire Department.
- F. On-site lighting shall be installed and maintained along all vehicular access ways and major walkways. Such lighting shall be directed onto the driveways and walkways within the project and away from adjacent properties. Lighting shall also be installed within all covered and enclosed parking areas. Light fixtures should be limited to a height of 14 feet and the fixture style and design should be compatible and consistent with the building design. Lighting in sensitive areas shall be timer controlled.
- G. Common open space shall be provided which is accessible and available to all project residents for outdoor activities in accordance with the Zoning Ordinance. Surfacing of the areas shall be any combination of lawn, paving, decking, concrete or other serviceable dust-free surfacing with a slope of not more than 10%. Landscaping shall be provided for screening, shade, and as a general amenity. Required private open space may not include public or private streets, driveways, utility easements, walkways, parking spaces or other areas primarily designed for other operational functions. Required open space located at ground level shall be screened from view of abutting streets by a wall, fence or dense landscaping.

2. Downtown Residential Development

The following design standards shall apply in the Downtown Residential District.

- A. Residential structures on interior lots shall appear as single-family dwellings from the street, with only a single entry plainly visible from the street frontage. Entries on structures located on corner lots shall be located and designed so as to minimize the structure's multi-family appearance.
- B. The roof slope design, door and window styles, setbacks, building materials and colors, and other architectural elements of any structure shall be compatible with and evocative of designs generally found in the surrounding neighborhood.
- C. Common parking areas shall not be visible from the street.

- D. Covered parking facilities shall be of a design compatible with the units. Garages and carport structures should not be located so as to conflict with building entries or outdoor use areas.

3. Hillside Residential Development

These residential hillside development design standards are intended to implement the goals, objectives and policies of the General Plan relating to the preservation and maintenance of the natural character and visual amenities of hillsides as scenic resources of the City. These standards apply to all new residential development in hillside areas.

A. Views

Because hillside development is more visible, measures should be taken to provide for a more pleasing appearance through site planning, architectural form and landscape. Panoramic views from hillside roads and public places are as important to the character of the community as views of hillside development. Therefore, hillside development shall minimize obstruction of views providing, at minimum, view corridors through site and off site.

B. Site Planning

- (1) Preservation and enhancement of significant visual and environmental elements that collectively express hillside character, including but not limited to ridgelines, significant vegetation and wildlife habitats, canyons, steep slopes, and important historical or cultural man-made features shall be comprehensively integrated within the hillside development.
- (2) Minimizing building setbacks, especially front and rear setbacks, may lessen the amount of required grading by reducing the overall width of the road and the building pad area. However, before reduced setbacks to a structure may be permitted, it shall be demonstrated that grading will be reduced, while still providing for useful private space as part of the site. Reduced setbacks shall not be used in order to enable maximized development of a site.
- (3) Lot lines shall be placed at the top of major slope areas within public view corridors to encourage their maintenance by the downhill owner.
- (4) Roads, driveways and lots shall be arranged and building envelopes placed to minimize grading.

C. Prominent Ridgelines

- (1) The site plan for a hillside development project should provide for the preservation of prominent ridgelines in their natural state, protecting them from development impacts, and maintaining them as a backdrop for development. "Prominent ridgelines" are those designated as such in the General Plan and which form a part of the skyline visible from any City arterial.
- (2) Significant views of prominent ridgelines shall be maintained from elements of the general circulation plan and other public open spaces, especially scenic highways.

D. Building Design

- (1) General building massing should reflect, respect and blend with site topography. Building forms shall be scaled to the particular environmental setting so as to complement the hillside character and to avoid excessively massive forms that detract from the hillside character.
- (2) Building facades should use plane changes or overhangs to create shadow line to further break up massive forms. Buildings on slopes shall step back or down with the topography.
- (3) A majority of the roof pitches within a project should be placed to angle with the slope. Collective mass rooflines shall reflect the naturally occurring ridgeline silhouettes and topographical variation, or create an overall variety that blends with the hillside. Totally flat rooflines shall be avoided.

- (4) On ridgelines not designated as prominent and used for continuous development along the ridgetop, collective roof lines shall be kept low in profile, and the natural silhouette of the ridge should be repeated with smooth transitions in height from building to building. Consideration of the ridgeline treatment may take variations in landscape features into account.
- (5) Building colors, materials and textures shall be selected to blend with the natural colors and hues of the surrounding hillsides.
- (6) Roof materials shall be rough textured, fire retardant material. Roof colors should be of darker tones, including browns, black, greens and terracotta. Bright or light colors should be avoided.
- (7) Where fences and walls occur on privately owned property within slope areas, their designs should be as uniform as possible.
- (8) Continuous rear yard fences and walls across the tops of slopes should be coordinated in the design and use of materials.
- (9) Retaining walls should be designed with smooth, continuous lines that conform to the topography. Retaining wall structures used to accommodate a patio or terrace should conform to the natural hillside profile as much as possible. Excessively high retaining walls (generally over 6 feet in height) shall be avoided. Terraced or stepped grading is preferred.
- (10) In deck construction, the distance between structure and grade should conform to the natural hillside profile as much as possible. Excessively large distances (generally over 8' in height) between structure and grade shall be prohibited. Deck skirting and the use of a landscaping transition to grade shall be encouraged.

E. Landscape Design

- (1) All cut-and-fill slopes within street rights-of-way or on land held by a homeowners association, and all slopes over five feet in height shall be landscaped with a combination of groundcover, shrubs and trees by the developer prior to the issuance of any occupancy permit. Planting of slopes shall be such as to prevent erosion.
- (2) Turf that requires mowing shall not be used on slopes in excess of 4:1.
- (3) Plant materials used in slope planting shall be fire retardant, drought tolerant introduced species or native plants adapted to and suitable for providing vigorous rooted growth.
- (4) In cases of severe soil problems where shrubs and tree pits can trap water leading to slope failure, hydroseed mixes or smaller plant materials will be required.
- (5) Plant materials on slopes should be placed in informal masses to help alleviate the impact of uniform, graded benches.
- (6) Shrubs planted on slopes should be arranged in broad, informal masses of the same plant materials. These masses should be combined to produce a mounded, textured slope surface, similar to the natural chaparral and woodland vegetation.
- (7) Trees planted on slopes shall be arranged in informal masses. These masses should be placed to selectively allow views from housing while partially screening buildings and reducing the scale of long, steep slopes.
- (8) Street trees should be selected which create an intimate scale along the streetscape and do not overpower narrower streets.
- (9) The arrangement of street trees should be carefully planned to create an informal character and to enhance potential views.
- (10) Street trees along roads without adjacent development should be selectively located to provide an edge to the road. They shall not be uniformly spaced, but used in-groups to

enhance and frame views. It may be appropriate to eliminate trees along roads through natural open space areas.

- (11) Planting along the slope side of development should be designed to allow controlled views outward, and to partially screen and soften structures.
- (12) In situations where edge planting at lower levels of slope development may block views from above, height restrictions on plant materials should be applied.
- (13) Planting design shall incorporate firebreaks in the form of natural looking open areas. Other fire prevention techniques shall be included in the landscape design as necessary.

F. Circulation

- (1) Circulation shall be aligned to conform to the natural grades as much as possible. Long stretches of straight road shall be avoided by utilizing gentle horizontal and vertical curves.
- (2) Roads that run parallel to the line of slope shall be avoided in order to reduce grading and to aid in drainage. The use of retaining walls is encouraged to eliminate unnecessary cut and fill.
- (3) Bridges shall be preferred for roads crossing drainage ways and canyons of exceptional environmental value to eliminate excess fill when structural requirements do not negate the intent of environmental preservation.
- (4) Pedestrian circulation traversing man-made slopes may be provided in benches; on low slopes, vertical shortcuts may be developed with steps.
- (5) Opportunities should be created for public views from roadways and public open spaces by selective placement of structures at key locations.
- (6) Common drives in single-family developments should be encouraged if grading is reduced by their use.

Section 10 Commercial Development

Part A: Downtown Commercial Development

The following design standards are intended to maintain and enhance the downtown area's pedestrian scale and historic architectural character.

1. Façade Design

- A. The basic storefront design should include large display windows with thin framing, a recessed entrance, a cornice or a horizontal sign panel at the top of the store front to separate it from the upper façade, and low bulkheads at the base to protect the windows and define the entrance.
- B. Building walls adjacent to streets, regardless of height, shall continue in one plane vertically from ground level to the parapet without significant setbacks, or projections.
- C. There should generally be more glass and less wall at ground level, balanced by more wall and less glass on the upper façade.
- D. Inappropriate historical themes shall be avoided.

2. Street-Level Transparency

- A. At least 80% of the total width shall be transparent and devoted to entrances and show windows or other displays that are of interest to pedestrians. Where a substantial length of windowless wall is found to be unavoidable, eye level display, a contrast in wall treatment, an offset wall line, decorative features, outdoor seating or landscaping shall be used to enhance the wall's visual interest.
- B. Clear, untinted glass shall be used. Mirrored, highly reflective glass or densely tinted glass may only be used as an architectural or decorative accent.

3. Roofs

Roof forms shall not be dominant elements and shall generally be concealed behind parapet walls. Mansard or other applied or implied roof forms are inappropriate.

4. Building Materials

- A. Building exterior finishes on the street elevation may include but are not limited to brick masonry, textured stucco, enriched concrete, tile or clay, or painted concrete block. Other materials may be used on upper elevations provided that they are well integrated with the street elevation materials.
- B. Awnings shall not extend more than four feet from the face of the building. Reduced awning extension may be required in areas where streetscape elements or sidewalk width limit the space available.

5. Building Colors

- A. For renovated structures, the historic color palette for the building should be used. This may be determined by review of historic photographs and paintings, analysis of residual surface paint and observation of similar architectural design styles in other locations. In addition, colors selected should be compatible with adjacent structures.
- B. For all structures, a full range of natural earthtones, pastels and their gradations are recommended. Generally, a single base color should be used, against which complementary accent colors are used to articulate facade details. Extremely dark and primary colors as a background are not recommended due to their susceptibility to fading and their inability to complement the desired muted palette.

6. Infill Development

- A. The building shall completely fill the site's street frontage(s).
- B. The new facade shall incorporate rhythms and proportions of windows, doors, storefronts and column spacing which are similar to existing structures. The composition of the infill facade (i.e., the organization of its parts) shall be similar to that of surrounding facades.
- C. Materials, colors and surface textures which complement existing structures shall be used.

7. Renovation

- A. Renovation and preservation of existing significant structures shall be encouraged. Renovation of existing structures should respect original architectural design details representative of significant historic design idioms, where present, and avoid the introduction of new or artificial design styles or elements.
- B. Retain elements that establish the structure's design style and character (e.g., storefront bays, windows, recessed entries, masonry surfaces, facade textures and details).
- C. Articulate cornices, parapets, belt courses, piers and columns, window and door frames, clerestories and other facade details by the use of differentiating colors or materials.
- D. Restore design elements that may have been partially or completely lost. Such may be facilitated by the review of historic photographs and comparable buildings in the City.
- E. Remove signage, lighting and other applied surfaces and materials that cover significant details.
- F. Integrate new signage, lighting, awnings and other facade elements with the design details.
- G. Integrate and screen new mechanical equipment so that it is compatible with the architectural character.
- H. Renovation of existing structures which do not exhibit significant architectural details should incorporate design elements which are compatible with and complement, but not replicate, adjacent structures. Conversion of a simple, contemporary storefront into an artificial design style should be avoided. Upgraded signage, a consistent graphics program, overhangs and canopies, lighting and planters should be used to upgrade the building's appearance and provide consistency in the downtown area.
- I. Upper stories and elevations of renovated structures should be treated with the same design sensitivity as the ground-floor elevation. Significant design details, such as columns and belt courses, should be continued vertically or horizontally. Materials, colors, signage, lighting and graphics should be consistent with the sidewalk elevation.
- J. The rear or side elevations of structures which are observable from abutting alleys, parking lots, sidewalks and streets should be upgraded to provide an attractive visual image.
- K. Where parking lots adjoin the rear of buildings, rear store entrances and passthroughs to the street are encouraged.

Part B: Plaza Commercial Development

In addition to the standards applicable to the Downtown Commercial Development, the following design standards shall apply to ensure compatibility with existing buildings and with the scale of the plaza's open space.

1. Building Height

- A. The following minimum cornice heights are encouraged for buildings fronting on the streets around the plaza in order to provide a continuous wall based on existing prominent structures, and may be met by extending the parapet. These heights may be exceeded if the additional height is set back from the building face.

Plaza Street:	25 feet
Center Street	30 feet
Matheson Street:	35 feet
Healdsburg Avenue	40 feet

2. Upper Level Openings

Upper level openings should provide generous views of the plaza, and provide some visual connection between the open space and upper floor activities. This can be achieved through large, well-proportioned windows that puncture the building wall, and with balconies that provide visual interest and variety. Large expanses of uninterrupted glass are not appropriate to the scale or character of the plaza. Individual window awnings, articulated window openings, and delicately detailed cornice lines should be used to give further interest and scale to the plaza elevations. Awnings shall not extend more than four feet from the face of the building.

3. Building Materials

Acceptable building materials shall include but are not limited to stucco, clapboard and light-colored masonry. In addition, building materials should be compatible with adjacent structures.

4. Corner Building Orientation

Corner buildings shall be oriented towards the plaza, with the principal entry opening onto the plaza or the corner.

Part C: Service Station Commercial Development

Service stations shall be designed so that form and scale are integrated with adjacent development as well as the general character of the area. In addition to the requirements found in the Zoning Ordinance, the following should apply:

1. Location of Activities

All activities and operations, including the scale of new and used merchandise, shall be conducted entirely within the enclosed service building.

2. Landscaping

Landscaping shall comprise a minimum of 10% of the service station site area and shall be required between all paved areas and the sidewalk or curb wherever driveways are not located.

3. Service Bays

Openings of service bays shall not face abutting public streets and shall be designed to minimize the visual intrusion onto abutting properties.

4. Lighting

All light sources, including canopy, perimeter, and flood shall be shielded or recessed within the roof canopy so that the service station is indirectly visible and light is deflected away from adjacent properties and public streets. Lighting shall not be of such a high intensity as to cause a traffic hazard or adversely affect adjoining properties. No luminaire shall be higher than 14 feet above finished grade.

5. Peripheral Wall

Where a service station adjoins property in a residential zone, a six-foot high solid wall or fence shall be constructed. The height of such wall or fence shall be determined from the residential property. Materials, textures, color and design of said wall shall be compatible with on-site development and adjacent properties. When the wall reaches the established front-yard setback line of a residentially zoned lot abutting the service station, it shall decrease to a height of 30 inches.

6. Restroom Screening

Restroom entrances viewable from adjacent properties or rights-of-way shall be concealed from view by planters or decorative screening.

Section 11 Mixed Use Development

The following guidelines shall apply to the design of projects that combine residential and non-residential uses in order to promote internal compatibility.

1. Residents' privacy

The project design shall incorporate features that protect the privacy of project residents to the extent feasible.

2. Residential access

Site planning and building design shall provide for convenient pedestrian and vehicular access for project residents.

3. Loading

Commercial loading areas shall be separated from residential uses and shall be screened from view from the residential portion of the project to the maximum extent feasible.

4. Refuse and recycling

Areas for the collection and storage of refuse and recyclable materials shall be provided separately for residential and non-residential uses.

5. Lighting

Lighting for non-residential uses shall be appropriately shielded to limit impacts on residential uses.

6. Noise and vibration

Noise impacts associated with non-residential uses, including noise-generating activities and equipment, shall be minimized. Residential uses shall be designed and constructed to minimize adverse impacts from non-residential noise and vibration.

7. Odors

Residential uses shall be protected from odor impacts associated with non-residential uses to the extent feasible.

Section 12 Industrial Development

1. Purpose

Design Review of industrial development is intended to encourage the harmonious appearance of buildings and sites, to ensure that new and modified uses and development will be compatible with the existing and potential development of the surrounding area, and to produce an environment of stable, desirable character.

2. Applicability

These Design Guidelines shall apply to the design of all development and improvements in the M-L Limited Industrial District, the M-G General Industrial District and the M-C-I Mixed Commercial and Industrial District. Development on Old Redwood Highway between Highway 101 and Bailache Ave. shall conform to the Design Guidelines for Old Redwood Highway in terms of the areas of concern specifically covered by those provisions; however, these Design Guidelines shall apply in all other respects.

3. Scope of Design Review

The following areas shall be considered in the design review process:

A. Site Planning

- (1) Appropriateness of setbacks, site coverage and building heights.
- (2) Adequacy of parking and vehicular and pedestrian circulation.
- (3) Relationship to abutting or adjacent sites and land uses.
- (4) Preservation of natural site amenities.
- (5) Use of landscaping.
- (6) Use of outdoor lighting and furniture.

B. Architecture

- (1) Scale, mass, bulk and proportion.
- (2) Compatibility with the desirable qualities of the neighborhood and/or streetscape.
- (3) Compatibility of building materials and details for all structures within a project.
- (4) Relationship to other structures, parking and accessory elements of a site.
- (5) Screening of utility and mechanical features.
- (6) Design of fences and walls.

C. Signage

- (1) Appropriateness of sign design, color and materials.
- (2) Relationship to building(s), site and landscape elements.
- (3) Lighting of sign(s).

4. General Design Standards

While encouraging the broadest possible range of individual and creative design, the following standards shall apply to all development.

A. Relationship to Existing Development

- (1) New development is not necessarily required to match surrounding existing development; however, in areas with a distinct character, compatibility with such design elements as setbacks, massing, materials, colors, roof pitch and wall openings is encouraged.
- (2) The siting and orientation of buildings should be sensitive to the pedestrian or vehicular nature of the abutting street(s) wherever possible, considering the nature of the proposed use.

B. On-site Relationships

- (1) A project containing many buildings should consider the size and massing of all buildings in relationship to each other.
- (2) Multi-tenant projects should have a unified design theme, including the consistent use of architectural elements, materials, colors and textures. Additions and alterations should be consistent with and enhance the design theme.
- (3) Inaccessible yards and similar outdoor spaces that would tend to be ignored, thereby leading to the accumulation of trash and weeds, should be avoided.
- (4) Consideration should be given to the design of all of a building's elevations.

C. Preservation of Natural Site Amenities

Waterways, view, mature trees and tree groupings, and significant vegetation should be preserved and incorporated into development proposals. Such features should be considered as strong site design determinants.

5. Colors and Materials

A. Building Colors

Building colors should be compatible with the surrounding area and should reinforce the visual character of the environment of the proposed building. Generally, building colors should be subdued and not garish, and should not become "signing" for the building or site.

B. Building Materials

- (1) The use of building materials that reflect the "natural" character of the City, such as wood, stone and brick, is encouraged.
- (2) All exposed metal surfaces should have factory-applied finishes.

6. Openings

A. Entryways

- (1) Public entrances to buildings should be clearly defined and integrated with building and landscape designs. They should be architecturally detailed to serve as focal points and pleasant invitations to visitors.
- (2) Public entryways to centers, complexes and other multi-tenant projects should be defined through special landscaping or other type of design treatment.

B. Windows and Doors

- (1) The placement and detailing of windows and doors should be considered an integral part of the architectural composition. The location of openings should not be determined solely by interior requirements.
- (2) The color and tinting of glass should be compatible with the color of the exterior walls.

7. Utilitarian Design Elements

A. Utilities and Mechanical Equipment

- (1) All new utility lines on and serving a project site shall be underground. Above ground reduced pressure backflow devices and other required aboveground facilities shall be screened by landscape or other means.
- (2) Mechanical equipment and accessory utility facilities, including roof-top equipment, shall be screened from view or integrated with the design of the building. This requirement shall apply to the construction of new buildings and to any alterations of mechanical systems of existing buildings that results in significant changes in such roof-top equipment and appurtenances.

B. Trash Enclosures

Trash areas shall be concealed from public view.

8. Other Design Considerations

A. Roofs

When exposed to view, roofs are a significant element in establishing a building's appearance and character and should be an attractive part of its total design.

B. Exterior Lighting

Exterior lighting shall be stationary and deflected away from adjacent properties. The light source shall not be visible off-site. Free-standing light fixtures should be limited to a height of 15 feet and the fixture style and design should be compatible and consistent with the building design.

C. Handicapped Access

Handicapped access shall be provided in accordance with the requirements of Title 24 of the State Building Code.

D. Address Display

The address of all buildings shall be displayed in a manner acceptable to the Healdsburg Fire Department.

9. Impact Minimization

A. Enclosure of Activities

Wherever possible, all business, service and processing activities shall be conducted entirely within a completely enclosed structure, except for off-street parking and loading areas, permitted outdoor storage and displays, gasoline service stations, and utility substations and equipment installations.

B. Screening Adjacent to Residential Zones

Wherever industrially zoned property abuts a residential zone a solid decorative fence or wall, or other appropriate screening, shall be erected along the property line abutting the residential zone when the industrial property is improved.

10. Landscaping

Landscaping planning should be an integral element of a project's design. Sensitive landscaping helps buildings blend with their surroundings.

A. Landscape Design

- (1) Plans and specifications for landscaping, irrigation systems, tree preservation and slope planting for erosion control shall reflect use of the following:

- a. Planting material of a type, size and placement compatible with the project and surrounding land uses.
 - b. Sound soil preparation and planting practices, and,
 - c. Proper irrigation for healthy plant growth and maturation and the avoidance of the watering of buildings, public ways and pedestrian access.
- (2) Planting area soil should be tested and soil amendments should be incorporated into the soil to correct any deficiencies identified by the test.
 - (3) The scale and nature of landscape materials should be appropriate to the site and/or structure. Large-scale buildings generally should be complemented by large-scale landscaping. Landscaping of sites on major streets should include large-scale trees.
 - (4) The use of indigenous, native plant materials is encouraged.
 - (5) Live plant materials generally should be used in all landscaped areas. Bark should be utilized as mulch; not as a permanent form of ground cover.
 - (6) Landscaping should be used to separate parking areas from buildings, to provide a visual landscaped foreground for buildings and to enhance the perimeter of a project.
 - (7) Water-conserving landscape design is strongly encouraged.

B. Installation

All landscaping and planter areas shall be installed consistent with a project's approved landscape plan, prior to final building permit sign-off for the project.

C. Maintenance

Landscaped areas shall be continually maintained in good condition and kept clean and weeded. Dead or dying plant material shall be replaced consistent with the approved landscape plan. Plantings shall be maintained under optimum growing conditions.

D. Irrigation

All landscaped areas shall be served by an automatic irrigation system that includes an adequate backflow device. Sprinklers shall be spaced to assure complete coverage of a landscaped area. Water conserving irrigation design is encouraged.

E. Slope Planting

Slopes shall be planted with hardy, well-adapted and drought-resistant plants. Rooted cuttings of permanent ground cover plants shall be planted or the slopes shall be hydroseeded.

F. Open Space Areas

Landscape plans for open space areas containing significant natural vegetation should preserve existing vegetation to the greatest extent feasible and enhance them through supplemental planting with appropriate species.

11. Parking Design

A. Landscaping of Vehicle Accommodation Areas

- (1) Landscaped areas shall be distributed throughout the vehicle accommodation area to visually separate parking areas into small increments. The vehicle accommodation area means the area of a site used by vehicles for access, circulation, parking and loading and unloading activities.
- (2) A minimum of one tree shall be provided for every 10 parking spaces and located so as to visually disrupt long rows of parked vehicles (trees may be clustered). Canopy-type trees should be used to provide a relatively consistent tree cover that will shade the pavement and vehicles.

- (3) Where vehicles are to be parked immediately adjacent to a public or private street, a screen with a minimum height of 30 inches, measured from the finished surface of the parking area, shall be provided.
- (4) Landscaping shall be provided between a building and a contiguous parking or circulation area in a manner satisfactory to the Planning Commission.
- (5) All landscaping within vehicle accommodation areas should be contained within a brick or masonry planter box or by a concrete curb not less than six inches high.

B. Lighting

- (1) Public parking areas designed to accommodate three or more vehicles shall have lighting facilities capable of providing sufficient illumination at every point of the parking area.
- (2) Any parking area illumination, including security lighting, shall be so arranged as to reflect away from adjoining properties and rights-of-way.

C. Common Driveways

Driveways which provide vehicular access to more than one site are encouraged.

D. Location of Loading Spaces

- (7) Loading spaces required by the Zoning Ordinance shall be located immediately adjacent to the exterior wall of the building they serve or within the building.
- (8) Loading spaces shall not be located in any required front or side yard setback.
- (9) The location of loading spaces should consider potential impacts on adjacent uses.
- (10) Loading spaces should be so located and designed so that trucks are not required to back into a public street for ingress or egress. Truck maneuvering areas should be provided on-site where necessary to comply with this requirement.
- (11) Loading spaces should be concealed from public view.

12. Fences and Walls

The following standards shall apply to the design of fences and walls:

- A. All sides of perimeter fencing exposed to public view should be finished in a manner compatible with the building, property and neighborhood of which it is a part.
- B. Fences and walls shall be maintained in a neat and orderly condition
- C. No portion of a wall or fence shall be used for advertising or display.

13. Outdoor Storage and Service Areas

Outdoor storage and service areas shall be subject to the following design standards:

- A. All raw material, equipment and finished projects shall be kept in an approved storage area.
- B. Storage and service areas should be screened from off-site view wherever possible.
- C. Storage areas should be limited to the rear of a site.

14. Signs

- A. Signs should primarily identify the business, establishment or type of activity conducted on the same premises, or the project or service offered thereon.
- B. They should be restrained in size and design, and, as an identification device, should not excessively compete for the public's attention.
- C. A sign's materials, colors, textures, size, shape, height, placement and design should be harmonious with the building, property and neighborhood of which it is a part.

- (1) A monument design is required for freestanding signs. Such signs should have a maximum overall height of eight feet, including any base.
- (2) A sign's illumination should utilize the lowest possible level consistent with adequate identification and readability, and should be directed solely at the sign or be internal to it. External lighting sources should be unobtrusive in design.
- (3) A sign program shall be required for all projects. Such a program shall include exhibits showing the location, size, color, materials, lighting and orientation of all proposed signs and any existing signs proposed to remain.
- (4) Freeway-oriented signs are unacceptable.
- (5) No portion of a sign should extend above the building façade to which it is affixed.

15. Building Setbacks and Height

A. Setbacks Adjacent to Public Right-of-Way

Structures shall be set back from any public right-of-way a minimum distance of 15 feet. Improvement in this 15-foot setback area shall be limited to landscaping, curb cuts and associated driveways, and monument signs

B. Building Height

The maximum height of a building adjacent to a public right-of-way shall be determined by a 60 degree vertical angle beginning at the edge of the right-of-way and extending onto the property, as illustrated in the accompanying diagram, to a maximum height prescribed by the Zoning Ordinance. The minimum setbacks associated with the building heights defined by the 60-degree angle are shown in the attached table.

16. Related Standards

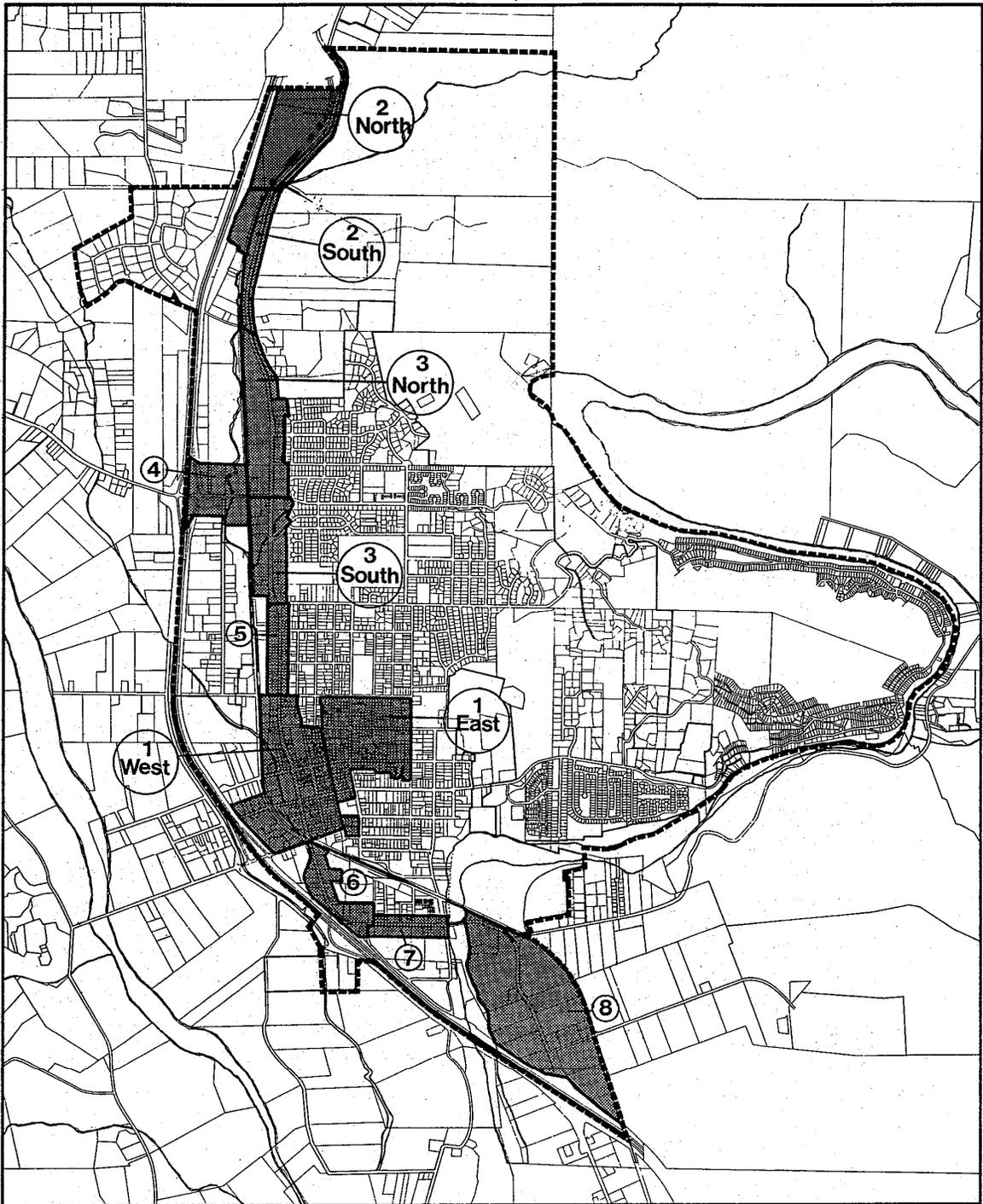
The following should also be referred to for further information regarding the design of industrial development:

Parking Design Standards (Section 3 of this document)

Old Redwood Highway Design Guidelines

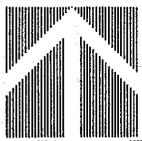
Section 13 Urban Design Districts

The City's General Plan calls for the establishment of Urban Design Districts to guide City project approvals and to upgrade the visual and aesthetic character of key urban areas. The Urban Design Districts are distinguishable physical and visual units of the city. The boundaries of the districts usually follow land use designations, property lines, or man-made features such as streets or railroad tracks. The following sections describe each of the Urban Design Districts and the key design guidelines recommended for each. A map for the district is at the end of each section.



**HEALDSBURG
CALIFORNIA**

J. LAURENCE MINTNER AND ASSOCIATES



URBAN DESIGN DISTRICTS

District 1 - Downtown

Downtown West

Subdistrict 1 West includes the commercial core of downtown, extending north to Grant Street, south to Mill Street, east to East Street and west to the railroad tracks. Also included is the Vineyard Plaza Shopping Center and the half block of service commercial zoned property to the north.

The key issue for the Downtown West district is to provide a coherent image for the downtown commercial core. Two of the elements that will determine a new character for existing development are facade improvements and streetscape improvements. Facade improvements should enhance individual building form. Buildings which have strong, classical architectural features such as ornate cornices or detailed trim should maintain and highlight those elements. Buildings whose features are not strong enough to make a statement by form alone should consider the use of special color or materials treatment. In general very modern building styles or materials would be inappropriate to this area. Specific streetscape features in this area are addressed in the Downtown Streetscape Plan. It is important for individual proposals to respect features found in this plan in regard to street trees, street furniture, awnings, signage and planters in order to avoid conflicts.

It is important to the character of the plaza for new commercial developments and redevelopments to face the plaza and orient their business activities to the plaza. Pedestrian access should be primarily from the plaza with secondary access from adjoining streets. In order to maintain the human scale, new developments should respect the plaza both in complimentary building design and streetscape design. Design elements should be pedestrian oriented. Signage above the storefront or awning should be limited and sidewalk level signage encouraged.

The design of the Plaza Hotel project is an important closure feature for the plaza. The design should create a strong image for the hotel itself and must contribute to the existing identity of the plaza architecture.

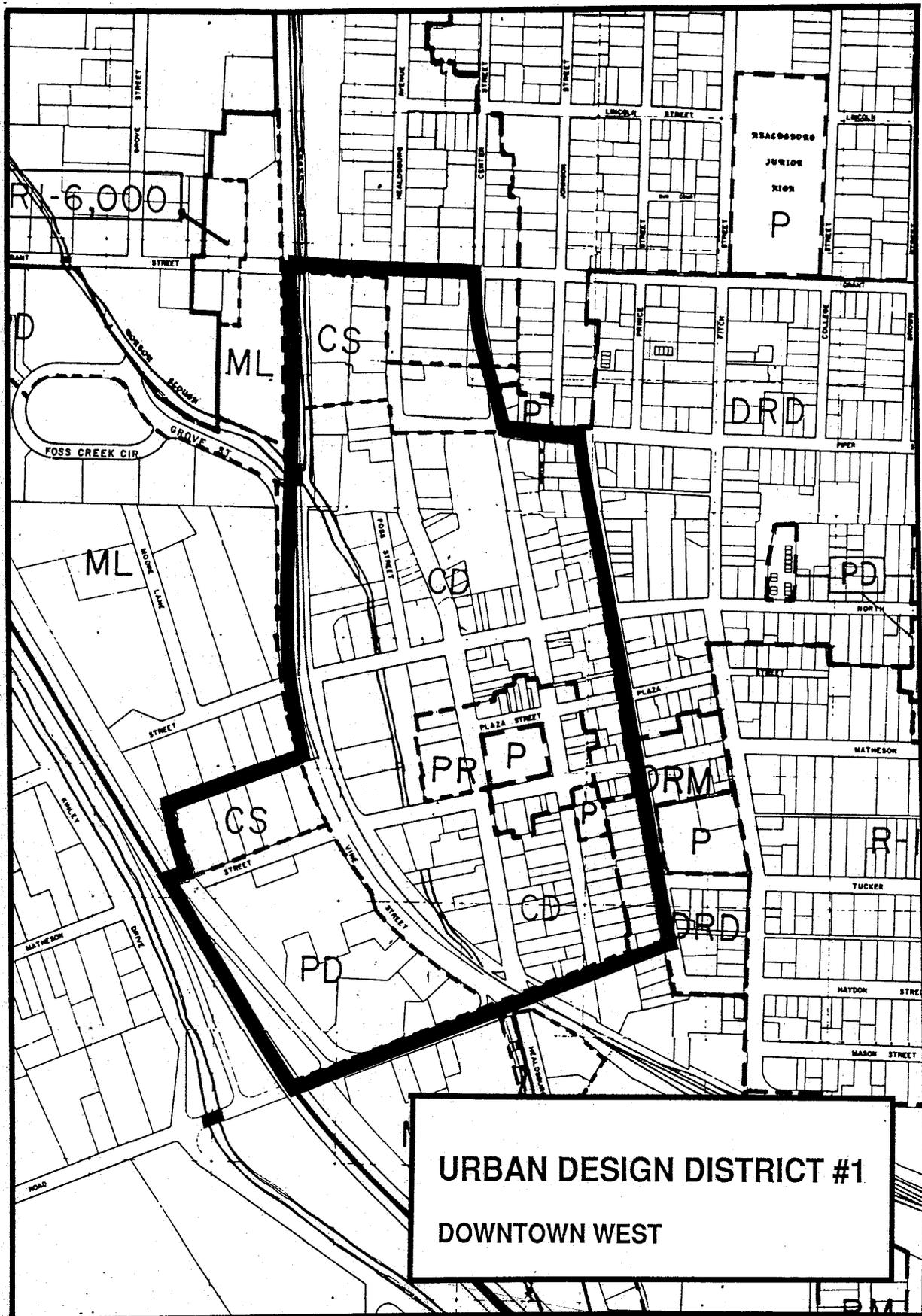
Border areas between Downtown West and Downtown East, where commercial property faces the residential areas, are particularly sensitive. Special façade and landscape treatments will be necessary to help these properties blend in visually with the residential area.

Other issues in Subdistrict 1 West include placement of parking areas and pedestrian access from parking to downtown and pedestrian access from residential areas. The Mitchell Center could be made a key east-west pedestrian link from the residential district to downtown. Any remodel of the Center should address this possibility, as well as the need for the Center to orient to both Center Street and Healdsburg Avenue.

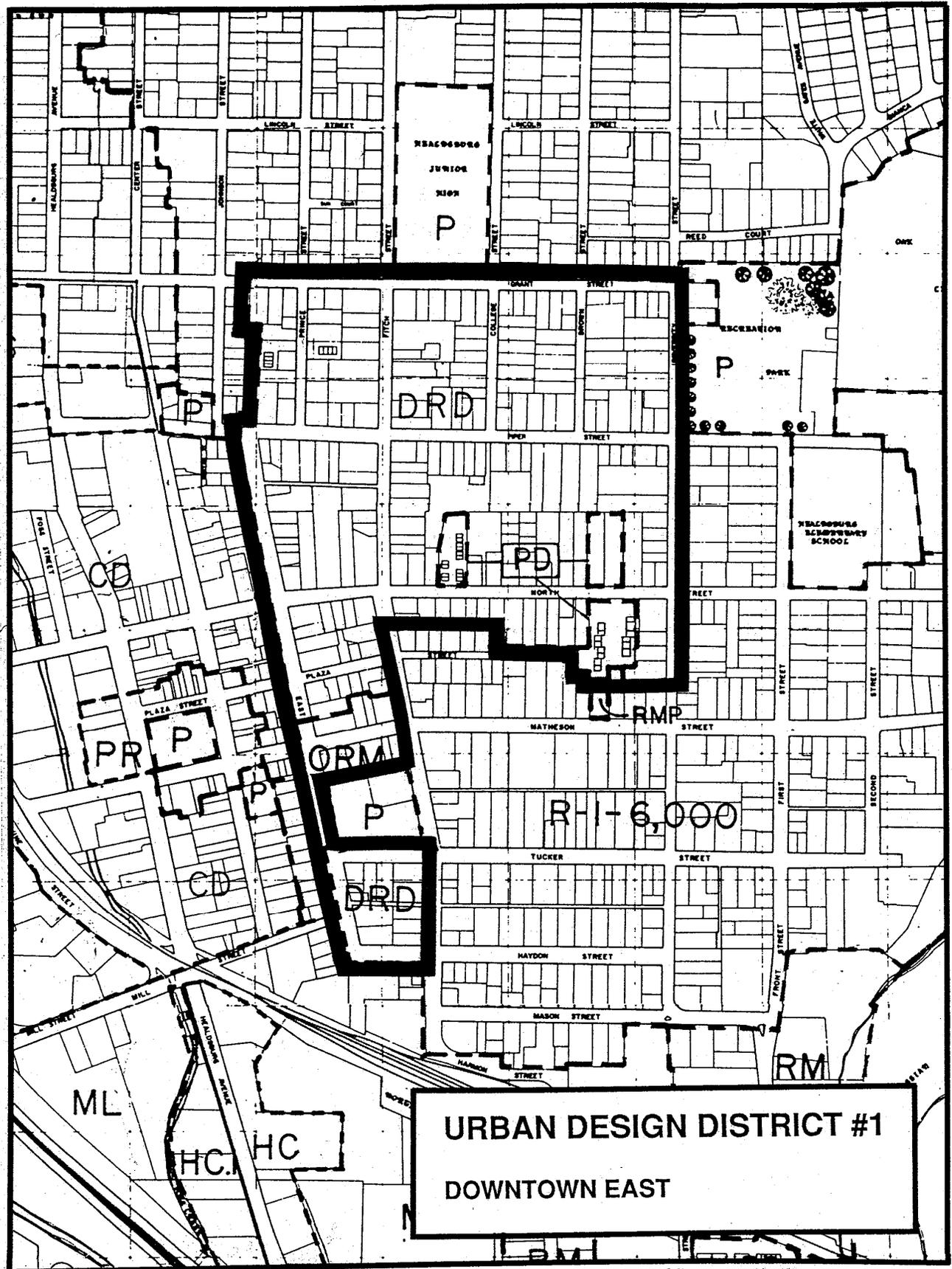
Downtown East

Subdistrict 1 East includes the area east of the downtown commercial core and is made up primarily of older residential development with many historic homes. This district has an Old World charm that should be respected and preserved by restricting contemporary structures and large expanses of paving. In-fill projects such as apartments or condominiums should incorporate roof slope, fenestrations and other features found in surrounding structures into the building design. Site planning and design of garage units is particularly important. Garage doors that face the street are a more suburban design feature that would not be appropriate in the older downtown neighborhoods. A regular occurrence of frontal facades with traditional front yard landscaping creates the streetscape character. This should be preserved and integrated with the architecture and landscapes of new projects.

Extensive on-street parking and restricted circulation due to narrow streets are existing conditions to consider when designing parking facilities. Parking design should provide easy ingress and egress points, good visual clearance to street traffic and adequate visitor parking areas. Of particular importance is the affect of parking areas on surrounding residences. Planting buffers, fences, wall or other measures may be necessary to maintain privacy for existing residences and diminish unsightly views.



URBAN DESIGN DISTRICT #1
DOWNTOWN WEST



URBAN DESIGN DISTRICT #1
DOWNTOWN EAST

Development of parking areas for residences converted to Bed and Breakfast Inns should follow the same standards of new development while still preserving the traditional appearance of the neighborhood.

There are many heritage trees along with other significant vegetation in the downtown residential neighborhood. Infill development projects should recognize the value of these trees in terms of contributing to the character of the neighborhood and the individual side. Standard tree protection measures for trees on the development site and adjacent to the site should be a condition of approval.

District 2 - Upper Healdsburg Avenue between the Urban Service Area Boundary and Chiquita Road

District 2 is divided into two subdistricts. Subdistrict 2 North includes all the BMC West Lumber Mill property and an approximately 200-foot strip along the east side of Healdsburg Avenue. Subdistrict 2 South includes the Simi Winery and a small-scale commercial development along the west side of Healdsburg Avenue and an approximately 200 foot strip along the east side of Healdsburg Avenue.

This district is the entry to the City from the wine region to the north. It will be important to provide for a pleasing visual image and sequence that will link the area to the downtown. The image for this area should remain rugged and natural as it is a transition area from rural to urban development. Simi Winery's existing design treatment should set the tone for new development in this important corridor. Landscape materials should include redwood, oak, evergreen groundcover and native stone with other materials introduced as needed to provide accent and color.

A wide landscape easement of at least thirty-five feet should be provided along Healdsburg Avenue in all new areas on the east and wherever possible on the west. A generous landscape easement will reduce the impact of views to the future residential development on the hillsides. The landscape easement should provide a consistent and strong edge along the highway without introducing the regular symmetrical plantings more typical to the downtown area. A fully developed bike trail should be provided on the eastern side of the highway within the landscape easement or road easement from the northern residential area into the commercial downtown area.

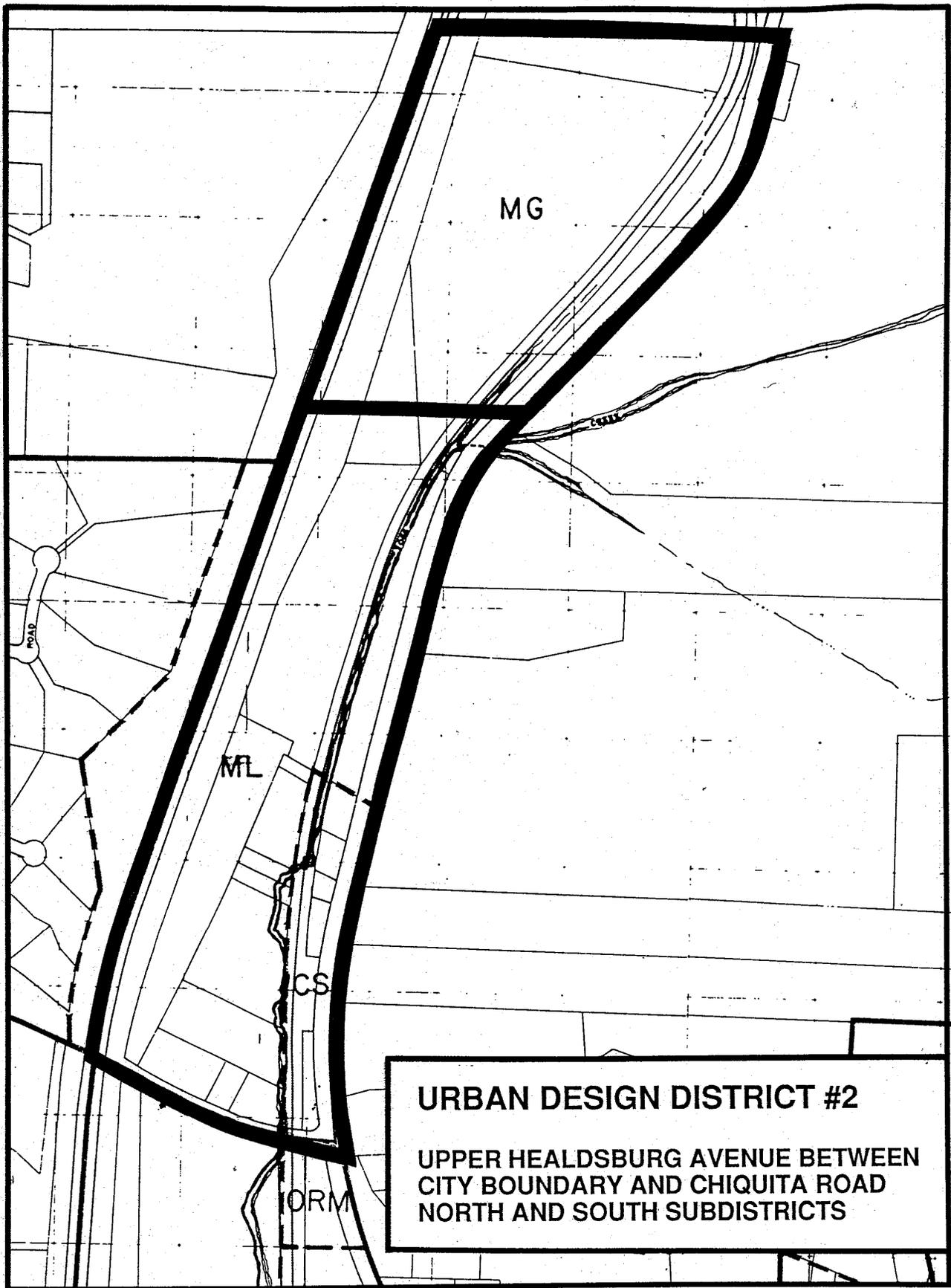
It may not be necessary to provide a regular street tree planting along residential streets within the north area. However, street landscape will be important. The hillside settings with curved streets may lend themselves to a more natural planting including native oak and redwood. Collector streets which connect to Healdsburg Avenue should have a more traditional street tree planting. Provisions must be made to preserve the native oak woodland wherever possible and to encourage the planting of appropriate regenerative specimens.

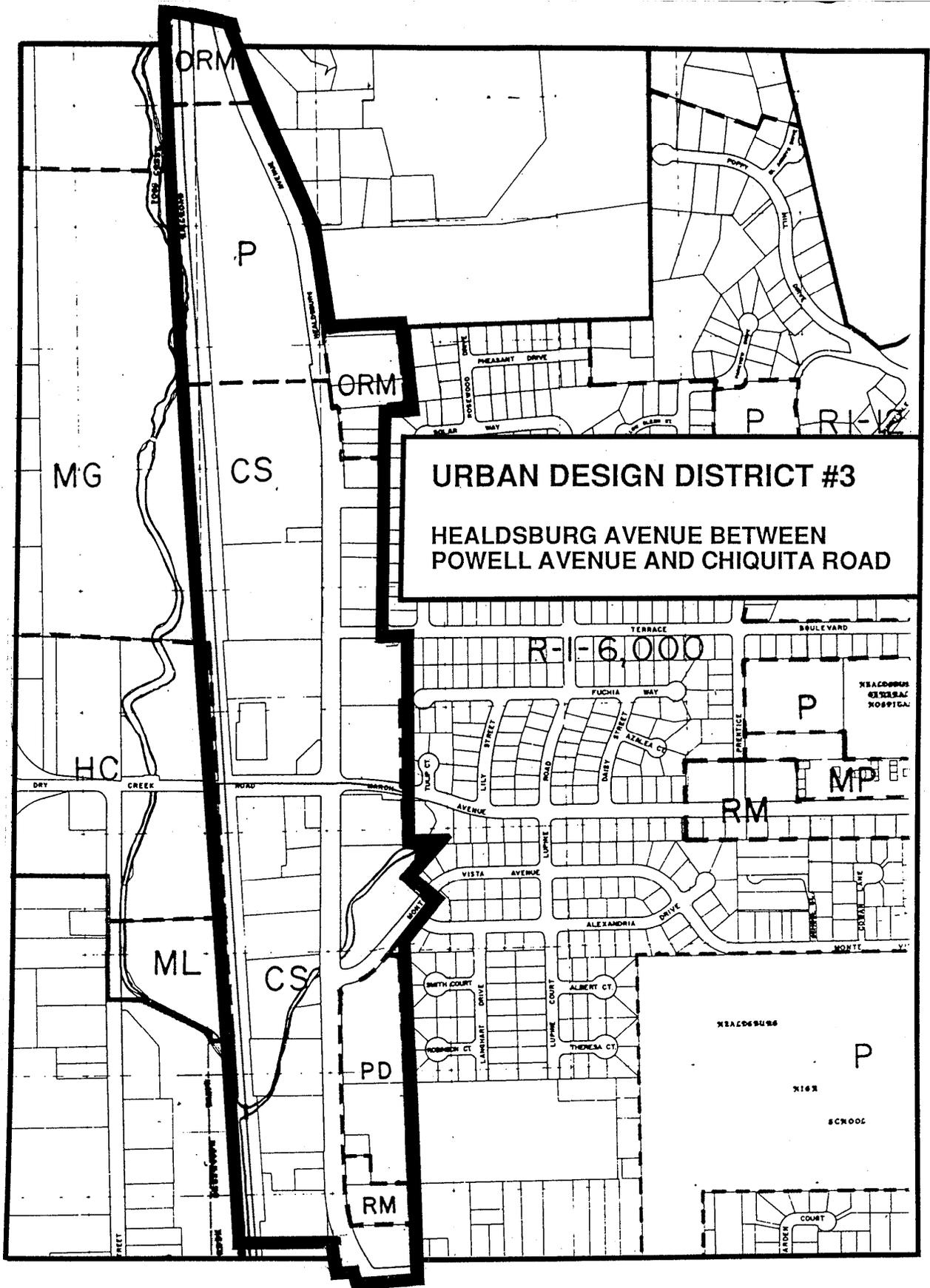
Other issues to consider in this district are the establishment of a gateway or city entry sign, the undergrounding of utilities, landscape or road treatment to slow traffic, and provision for pedestrian access to commercial areas to the south and downtown.

District 3 - Healdsburg Avenue between Chiquita Road and Powell Avenue

District 3 is divided into two subdistricts. Subdistrict 3 North includes undeveloped property, an auto dealership, the shopping center on the west side of Healdsburg Avenue at Dry Creek, and a 200 foot strip of largely undeveloped property including the new elementary school and strip commercial development on the east side of Healdsburg Avenue. Subdistrict 3 South extends from Dry Creek Road south to Powell Avenue and includes a mixture of land uses and vacant properties.

Due to the mixed zoning in Subdistrict 3 North and its location north of Dry Creek Road, a formal type streetscape is not appropriate. A transition from a strong natural landscape design to a more traditional avenue planting should occur between Chiquita Road and the Dry Creek Road intersection. This should be achieved by integrating avenue trees such as those found on Dry Creek Road with frontage landscape more typical of the Simi Winery plantings. Subdistrict 3 South should introduce a more traditional avenue type planting to guide one toward the downtown. In both subdistricts a strong pedestrian orientation is appropriate with a recessed sidewalk treatment and a parking strip/buffer between the street and the sidewalk.





Of particular importance is the effect of commercial development on surrounding residential areas. Site development and rear architectural elevations should be carefully planned to minimize intrusion on existing residences. Planting buffers, fences, walls or other measures may be necessary to maintain privacy for existing residences and diminish conflicts between uses.

Other issues to consider in this district are the establishment of directional signs leading to downtown; the undergrounding of utilities; and continuation of a bike trail to the downtown.

District 4 - Dry Creek Road Area

District 4 includes Dry Creek Inn and Adel's restaurant as well as older industrial and commercial uses along Dry Creek Road from the freeway to Healdsburg Avenue.

The key issue in this district is for establishing a quality visual image for the city freeway entry while still accommodating commercial freeway development. A framework for establishing a coherent image would include generous landscape frontage for all new developments. New landscape design should take cues from the existing off ramp landscape. Regular street tree planting should be introduced to visually tie this area to Healdsburg Avenue.

Signage should be of monument type not to exceed 8 feet in height and must be located to provide vision clearance for traffic. Sign materials should be consistent with those of the building. Well-designed directional and informational signage should be introduced to inform the freeway traveler of other services and destinations within the City and the region.

District 5 - Healdsburg Avenue between Powell Avenue and Grant Street

This district includes mixed office and residential uses fronting on Healdsburg Avenue and extending west to the railroad tracks.

This office and residential district is a buffer between two different types of commercial areas in Healdsburg, the northern service commercial properties and the downtown. The strong pedestrian character of mainly residential development or converted residential offices should be continued in District 5. It is important to maintain a human scale for the pedestrian. This should be accomplished by augmenting existing landscapes and by the addition of a parking strip/buffer between the sidewalk and the street where permissible. When planning new parking areas for converted residential offices, it is essential to screen parking from the street, provide safe access to the building and maintain a safe visual clearance to traffic on Healdsburg Avenue.

Signage shall be limited to 12 square feet per site and should not exceed 8 feet in height. Sign materials should be consistent with the building and with the residential character of the area. Signs shall not be directly illuminated.

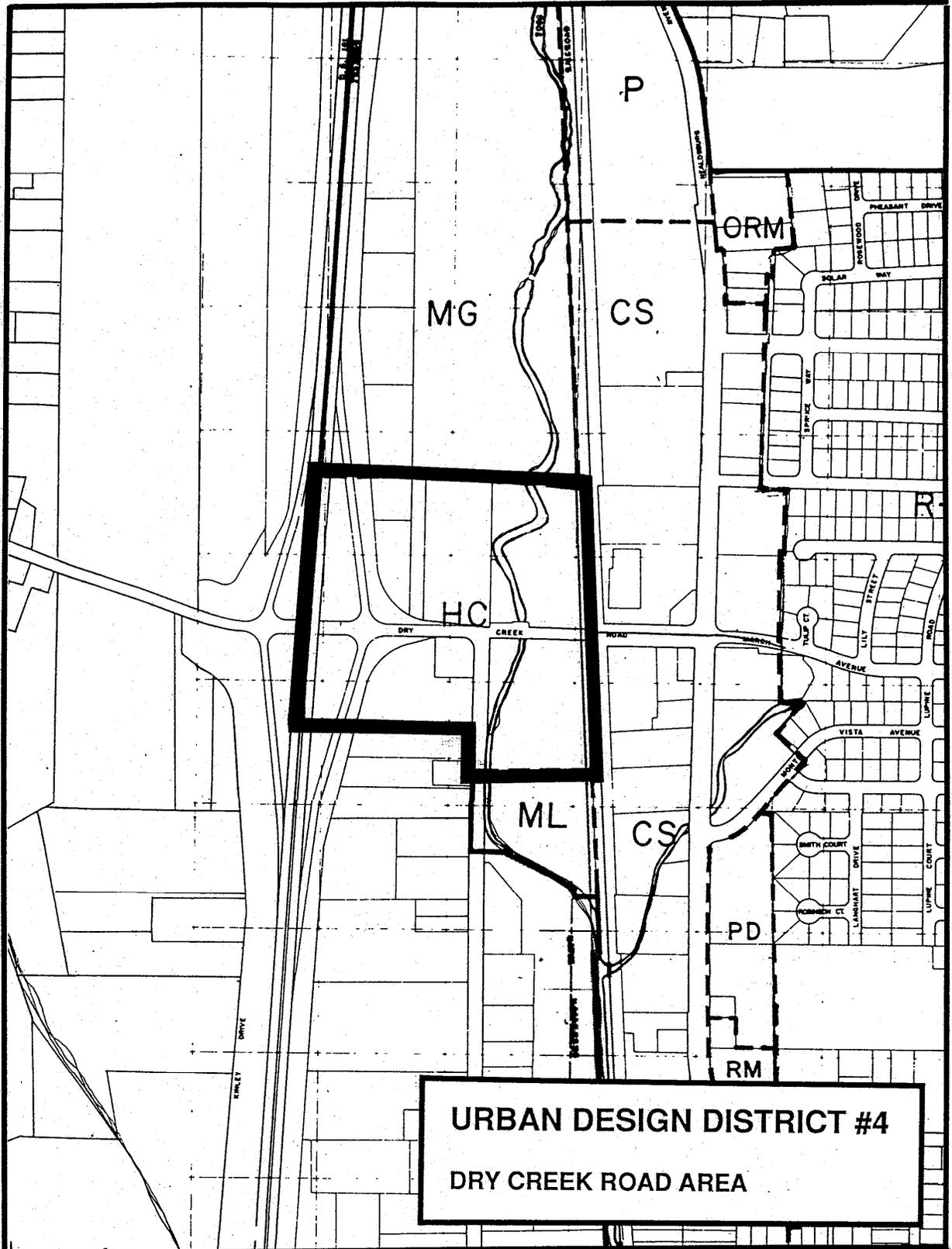
Other issues in this district are undergrounding utilities, landscape or street treatment to slow traffic, and the need for mid-block pedestrian crosswalks.

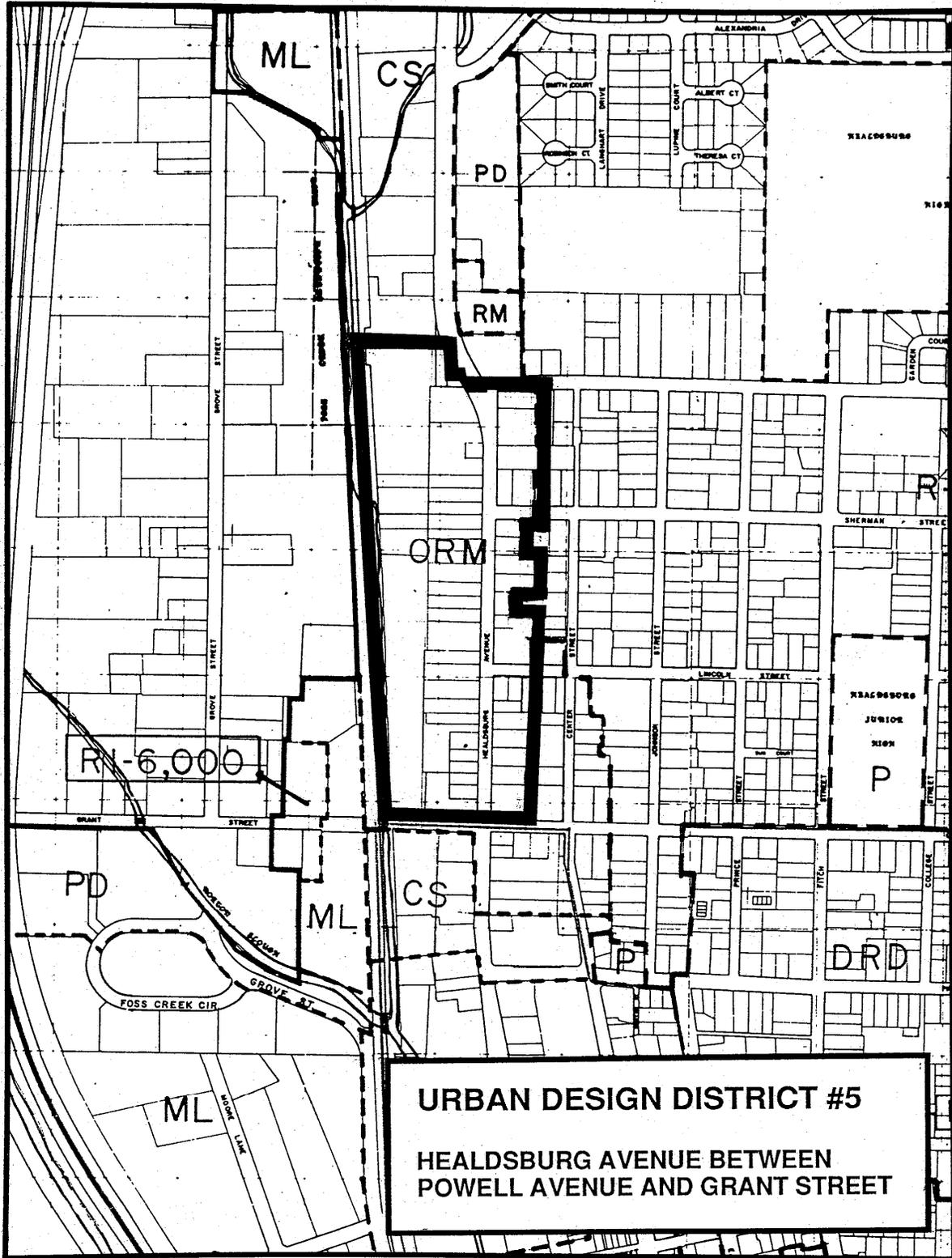
District 6 - Healdsburg Avenue between Mill Street and Adeline Street

This area includes a mixture of commercial and industrial development between Mill Street and Adeline Street.

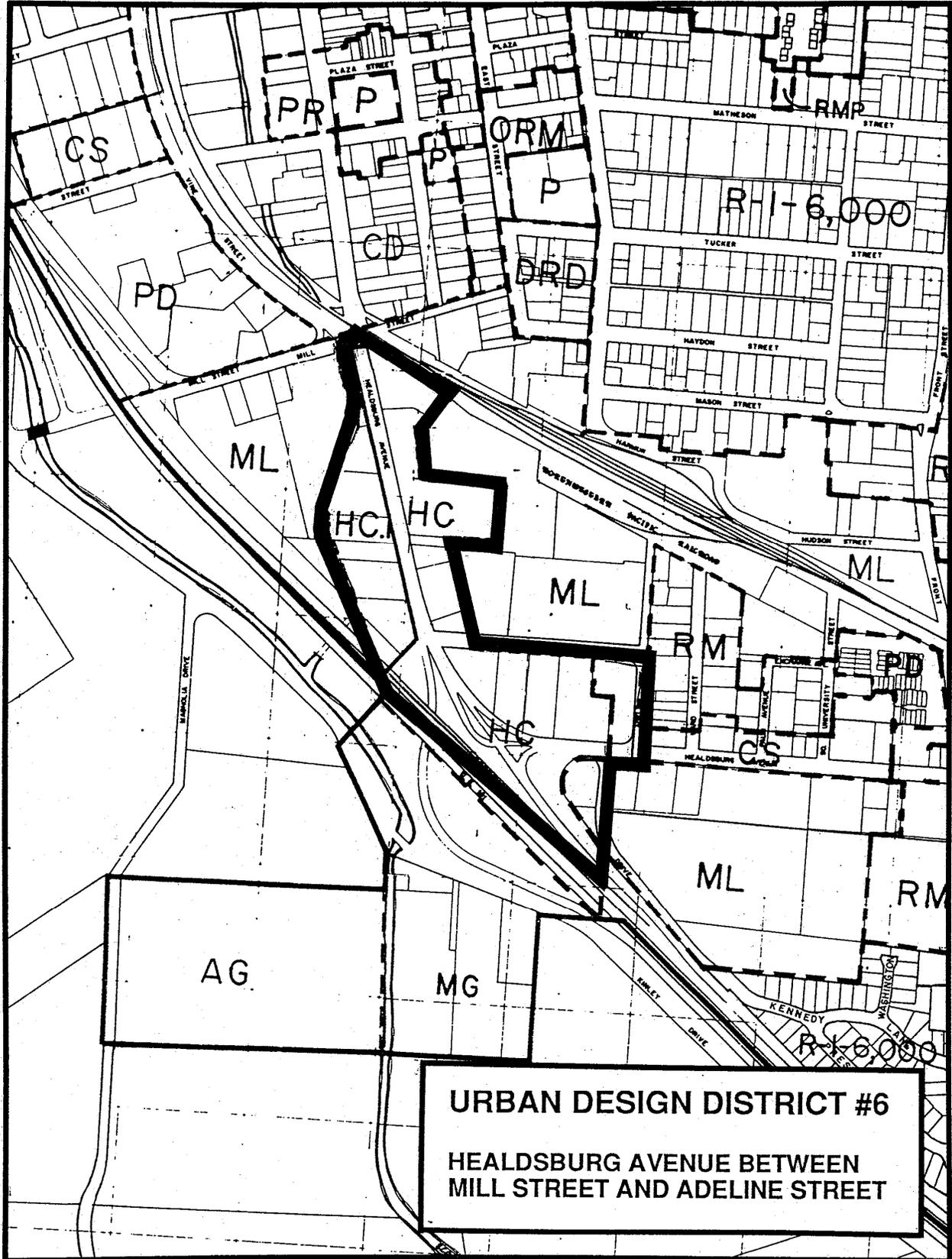
This is the south entry corridor into downtown from the freeway onramp. It is important to provide a pleasing and coherent visual image that has a strong visual link to downtown. Street trees should be introduced along both sides of Healdsburg Avenue to create an axial view to downtown from the freeway off-ramp. The off-ramp landscaping should be augmented. As new properties develop on the east side of Healdsburg Avenue, provisions should be made to improve pedestrian and bicycle access between the downtown area and the river.

Street sign controls will help alleviate a chaotic visual image in this area. Obsolete and non-conforming signs should be eliminated as properties are remodeled. New signage may be monument type not to





URBAN DESIGN DISTRICT #5
HEALDSBURG AVENUE BETWEEN
POWELL AVENUE AND GRANT STREET



exceed 8 feet in height or may be placed on awnings, painted on windows or mounted on facades. Colors and sign materials should reflect those of the building.

A well-designed directional sign should be introduced in this area to inform tourists and travelers of potential destinations within the City and the region.

District 7 - Healdsburg Avenue between Adeline Street and Memorial Bridge

This area includes a mixture of commercial, industrial, and residential uses and is bordered on the east side by the Russian River.

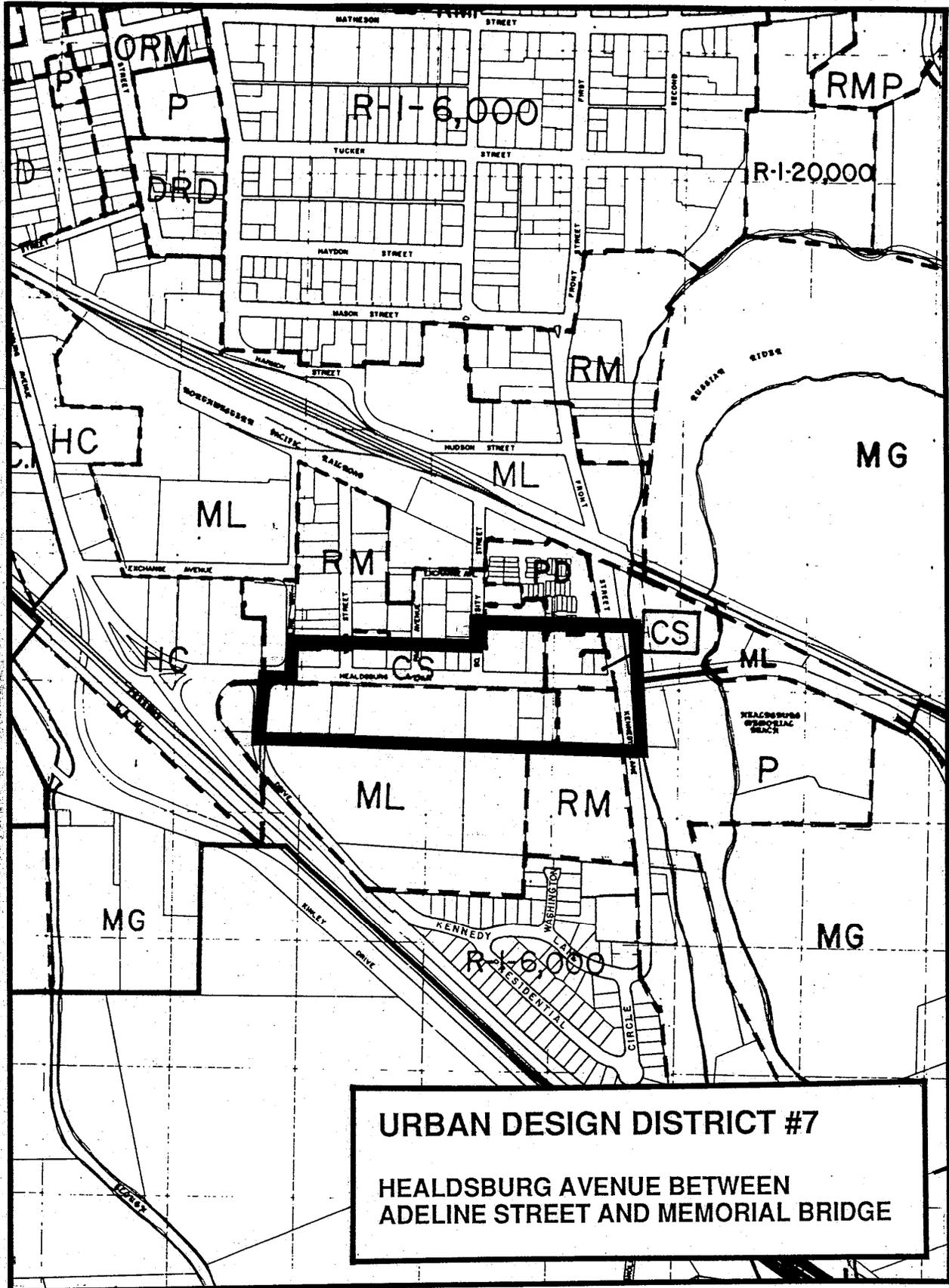
The Russian River and the Bridge are very distinct landmarks for the City of Healdsburg. Future bridge design should take into consideration safe pedestrian and vehicular access that slows traffic. In addition, it should incorporate a design theme that will provide the traveler with a sense of arrival. The design should reflect the character of Healdsburg.

A large landscape easement which provides a pedestrian link between the downtown and the river is another consideration. A parking strip/buffer should be included to separate the sidewalk from the street. The existence of the large palm trees helps provide a feeling of arrival. Future road alignment should incorporate them into the road easement as a single row of boulevard trees or into the landscape easement as a parking strip. A relocation of the palm trees may also be feasible.

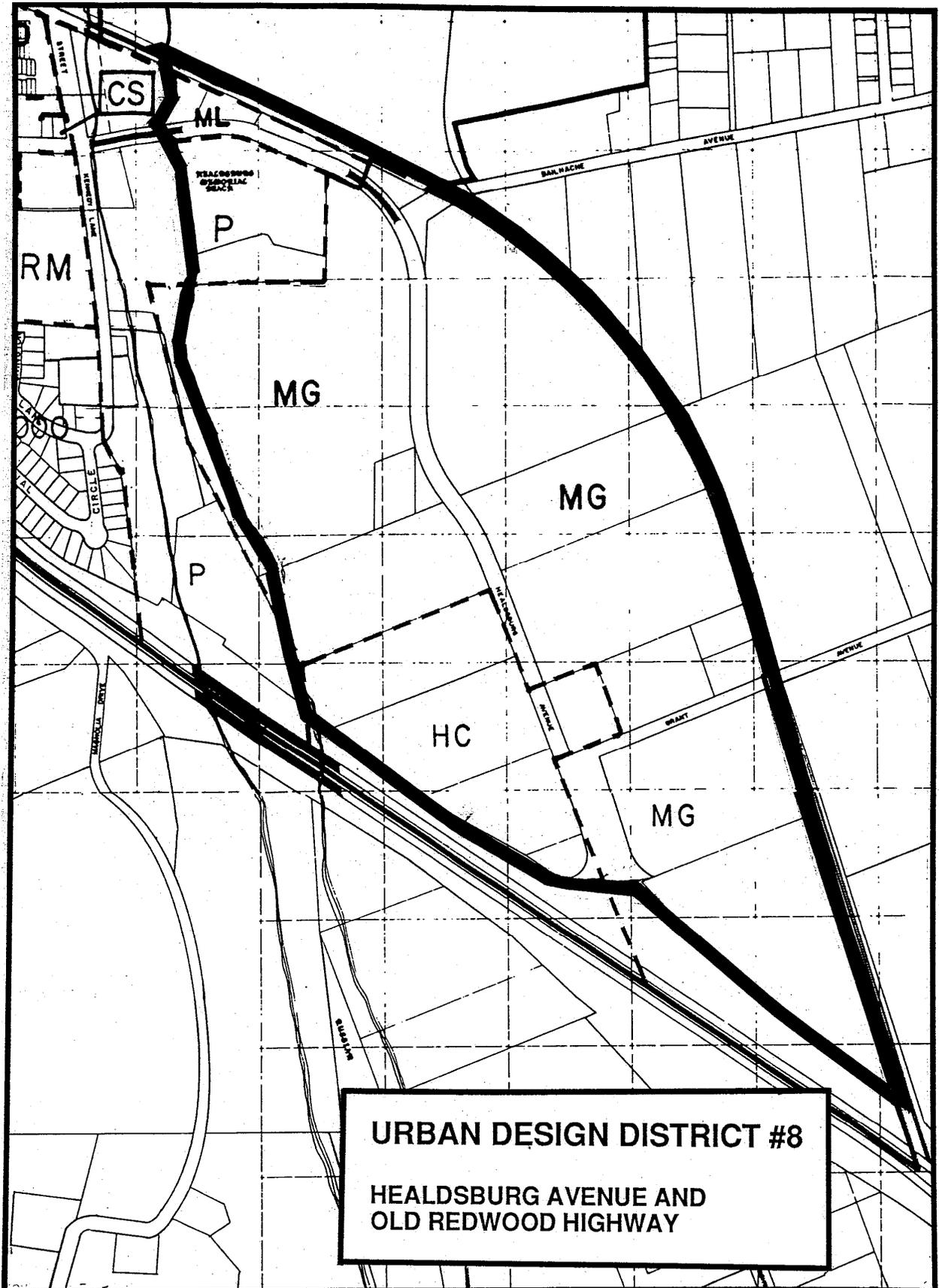
District 8 - Healdsburg Avenue/Old Redwood Highway

This district includes all of the Healdsburg Avenue/Old Redwood Highway area between Memorial Bridge and the freeway. It is designated for heavy industrial and highway commercial uses.

District 8 is subject to the Old Redwood Highway Guidelines and the Industrial Design Standards. Additional consideration should be given to design problems specific to the area, such as the screening of undesirable elements from view from Highway 101. The development of a gateway or city entry sign is another issue.



URBAN DESIGN DISTRICT #7
HEALDSBURG AVENUE BETWEEN
ADELINE STREET AND MEMORIAL BRIDGE



Old Redwood Highway Design Guidelines

Highway 101 to Bailhache Avenue

These design guidelines have been prepared to assist the property owners on Old Redwood Highway from the 101 Freeway to Bailhache Avenue to revitalize their street in accordance with an overall plan appropriate for the southern entrance into the City of Healdsburg. They seek to simplify and unify the streetscape through specific standards for building setbacks, landscaping, driveways, parking, signing and lighting.

These design guidelines have met with the approval of the City of Healdsburg City Council and Planning Commission. While they will not be a part of City codes or ordinances and will not have legal status, the Planning Commission will refer to them as a standard for project approval for this area.

A. Setbacks

1. Front Property Line Building Setback

- a. Thirty-five (35) feet landscaped building setback from property line should be maintained along Old Redwood Highway (see Figure A).

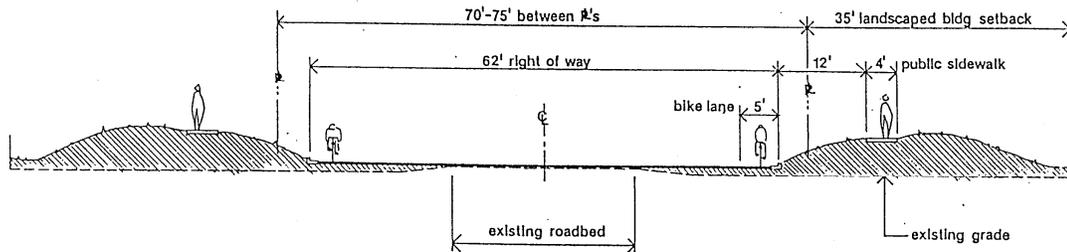


Figure A Streetscape Section

- b. The maximum height of a building at the setback along Old Redwood Highway should be limited to eighteen (18) feet. The height of a building may increase with a corresponding increased setback determined by a twenty-five (25) degree vertical angle beginning at the top of curb and extending onto the property. The Planning Commission may waive this standard when addressing existing buildings (see Figure B).

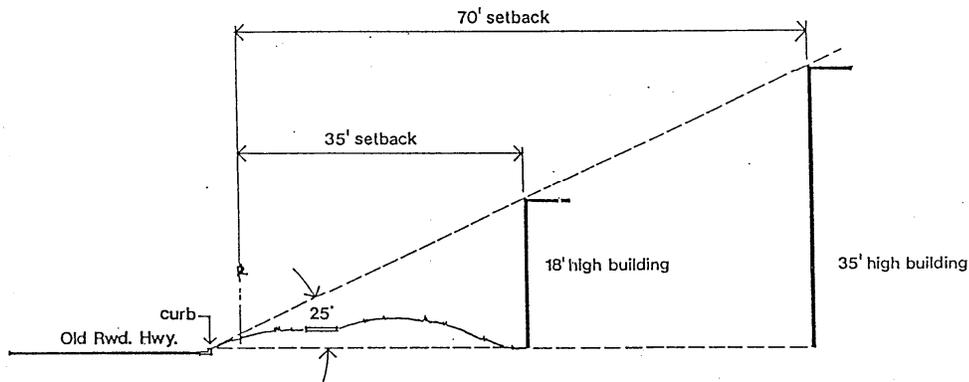


Figure B Building Setback Section at Old Redwood Highway

2. Side Property Line Landscaping

Ten (10) feet landscaped area along side property lines may be requested by the Planning Commission to screen objectionable views, uses and activities (see Figure C).

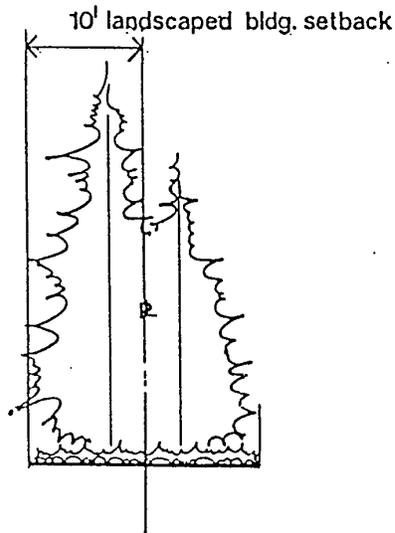


Figure C Building Setback Section - Side Property Line

3. Rear Property Line Building Setback

- a. Ten (10) feet building setback from rear property line is required by this ordinance.
- b. Two and one half times the height of the river bank plus a 30-foot building setback is required along the Russian River bank by the Sonoma County Water Agency. This setback is to provide an area for riparian habitat along the river and may require planting with indigenous plants to achieve this goal.

B. Landscaping

1. Landscape Materials

- a. Street Trees - Liquidamber Styraciflua 'Palo Alto' - Sweet Gum
15 gallon size; locate 6 feet behind face of curb at 30 feet on center.
- b. Buffer trees - Sequoia Sempevirens 'Soquel' - Coast Redwood
15 gallon size; locate within setback at 10 to 15 feet on center.
- c. Accent tree - Prunus Blireiana - Flowering Plum or Malus Floribunda - Flowering Crabapple
15 gallon size; locate at both sides of driveway in groupings of 4 to 5 trees.
- d. Focal trees - Salix Babylonica - Weeping Willow
15 gallon size; locate at street intersections, all corners, in groupings of 3 to 4 trees.
- e. Groundcover - Cotoneaster Dammeri - Bearberry
1 gallon size; locate throughout landscaped setback at 3 feet on center.
- f. Earth mound
Install continuous earth mound to 3 to 4 feet above top of curb within landscaped building setback. Side slopes of earth mound should not exceed 4 to 1.

- g. Mulch - One-inch to two-inch diameter Redwood bark.
Install to a thickness of 3 inches throughout landscaped area.
- h. Soil Preparation
Planting area soil should be tested by a soil testing laboratory. Soil amendments should be incorporated into soil to correct any deficiencies identified by the soil test.

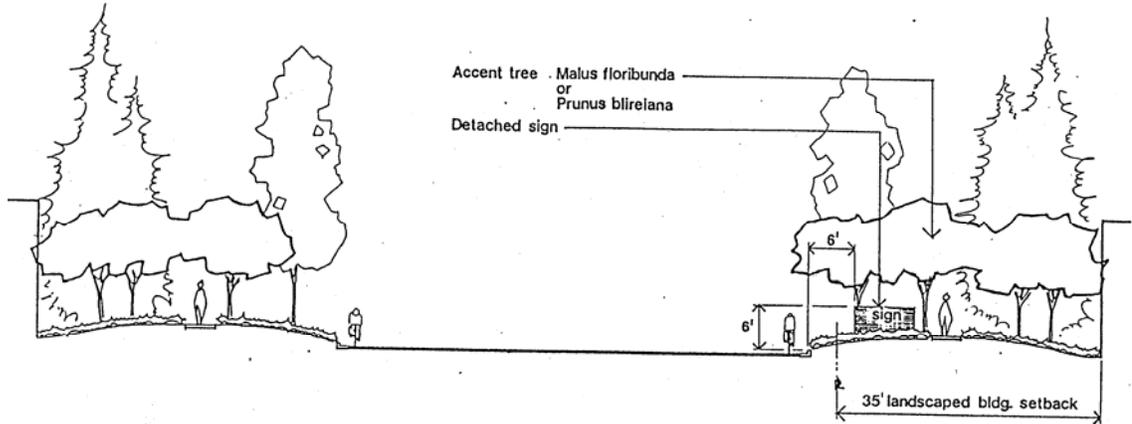


Figure D Streetscape Section

2. Irrigation

An automatic irrigation system should be installed for the entire landscaped area. The irrigation system should include an approved backflow device.

3. Maintenance

Landscaping maintenance should include pruning, cultivating, weeding, watering, and applications of appropriate insecticides and fungicides necessary to maintain plantings in optimum growing condition free of insects and diseases.

C. Property Access

1. Private driveway access to Old Redwood Highway should be limited to one driveway, when possible. A maximum of two (2) driveways may be allowed when proven necessary.
2. Private driveways should be a maximum of 35 feet wide.
3. Private driveways should be located a minimum of 200 feet apart, curb cut to curb cut.

D. Parking

1. Parking should not be permitted on Old Redwood Highway nor within the landscaped building setback.
2. All parking should be screened from Old Redwood Highway by a combination of earth mounding and landscape material.

E. Outdoor Storage

1. Outdoor storage should not be permitted within the landscaped setback.
2. Outdoor storage should be within a fenced yard, screened from Old Redwood Highway by a combination of earth mounding and landscape material.
3. The height of outdoor storage should not exceed the height limitation on buildings in respect to the setback from property lines (see Figure B).

F. Fencing

1. Fencing should be located behind the landscaped building setback along Old Redwood Highway.
2. Fencing along Old Redwood Highway should be limited to six (6) feet in height and constructed of wood and other natural materials such as concrete, aggregate, stone, brick or slumpstone.

G. Signing

1. One detached sign should be permitted within the landscaped building setback for the purpose of identifying the occupant of the property. The sign should be a low profile monument type sign located a minimum of six (6) feet from face of curb and should not exceed 6 feet in height above top of curb. The maximum message area should not exceed 32 square feet on each side. Wood and other natural materials such as concrete, aggregate, stone, brick or slumpstone are acceptable building materials. The sign should be externally illuminated with a light source shielded from Old Redwood Highway (see Figure E).
2. One attached sign may be located on each structure for the purpose of identifying the occupant of the building. The sign should be located flat against the vertical surface of the building below the roof eave with a maximum message area of twenty-four (24) square feet.
3. Fascia and roof-mounted signs, projecting signs, wall graphics and window signs should not be permitted.
4. Informational signs pertaining to directions should be a maximum of 6 square feet and 6 feet above the ground.
5. A coordinated sign program should be developed for each property and include detached monument sign, attached signs and informational signs.

H. Lighting

1. Lighting should be down directed and shielded so the light source is not visible off the street.
2. The maximum height of light fixtures should be below roofline of structures on property.
3. All but low-level lights should be energy efficient high intensity discharge lamps.
4. The light fixture style should be compatible with the building design.

I. Public Sidewalk and Transportation

1. A four-foot wide continuous public sidewalk should be installed within the setback along both sides of Old Redwood Highway. The sidewalk should be located on the side of the earth mound 12 feet from the face of curb.
2. Bus stop shelters should be provided at the intersection of O.R.H. and Grant Ave. and at the intersection of O.R.H. and Bailhache Avenue.

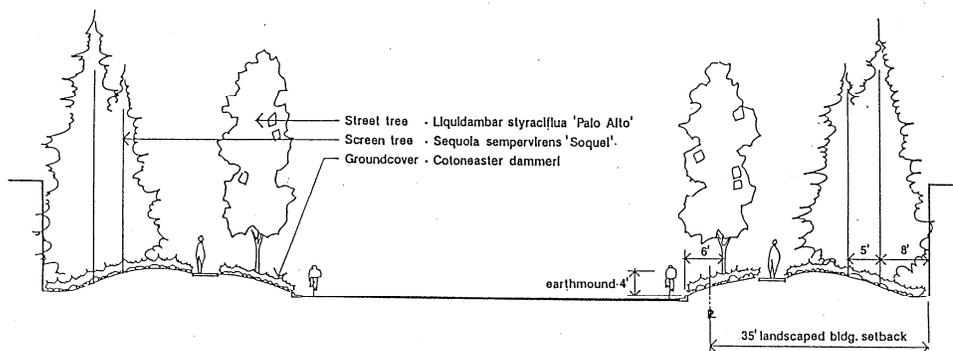


Figure E Streetscape Section

Appendix A

Design Review Procedural Guidelines

Purpose

The purpose of design review is as follows:

1. To preserve the natural beauty and attractiveness of new development where it occurs;
2. To promote and, wherever possible, improve the appearance of the community;
3. To ensure that indiscriminate clearing of property is minimized and the destruction of trees and shrubs is also minimized;
4. To obtain the best possible traffic circulation within the community;
5. To ensure that the design and layout of new development projects are compatible with the Healdsburg General Plan, applicable sections of the zoning ordinance and are in scale and character with surrounding land uses.

Design Review Applicability

1. The following applications are subject to minor design review by the Planning Director:
 - A. All signs and awning permits for projects not part of a larger project subject to design review by the Planning Commission.
 - B. Minor changes to the exterior of existing buildings that require a building permit.
 - C. A change of use in a building and site not involving substantial site changes. This determination will be made at the time of business license application.
 - D. Changes to non-residential site design not involving major structural or site changes or changes of use.
 - E. Construction of or an expansion greater than 500 square feet to a one-family dwelling on a site uphill and visible from a scenic highway or road, or located within a scenic ridgeline corridor designated by the General Plan as determined by the Planning Director.
2. The following applications are major design review applications, subject to review by the Planning Commission:
 - A. Commercial, industrial, office and residential projects (two or more residential units per site) which involve the development of vacant land with site and building improvements or involving major changes or additions to a previously developed site.
 - B. Applications involving a change of use that requires substantial changes to the site and proposals for exterior building modifications.
 - C. Projects subject to development approval by the Planning Commission, such as subdivisions, use permits and projects requiring the approval of variances.

Applications for Design Review

1. The owner or authorized agent of a site where a development or activity is subject to design review shall submit an application to the Planning Department to include the following:
 - A. A site plan, drawn to scale and fully dimensioned showing all property boundary lines, adjacent streets or alleys, the proposed layout of structures and other improvements including, where appropriate, driveways; pedestrian walks; off-street parking and loading areas; landscaped planters, fences and walls, trash enclosures and related information. The site plan shall indicate the location of entrances and exits and the direction of traffic flow into and out of parking areas, the location and dimensions of parking spaces and loading berths and all vehicular turning and maneuvering areas.

- B. A landscape plan, drawn to scale and fully dimensioned, showing the location and size of existing trees proposed to be retained on site, the location and design of landscaped areas and the varieties of plant material to be planted therein, and other landscaped features.
 - C. Architectural drawings, drawn to scale and fully dimensioned, showing all elevations of the proposed structure or structures, as they will appear upon completion of construction. All exterior surfacing materials and colors shall be specified. Cross sections through proposed construction areas and elevations of adjacent structures may be required by the Planning Department. Composite sketches from public vantage points may be required.
 - D. Detailed drawings depicting the design of site features and elements, such as fences, benches, light fixtures, retaining walls, special paving features and similar elements.
 - E. Concept grading plans, if construction is proposed on a hillside site and re-grading is required, including existing and proposed contours and a computer-generated illustration delineating proposed grading.
 - F. Scale drawings of all signs that are subject to design review, showing size, location, material, color and method of illumination (if proposed).
 - G. Fees as may be required.
2. Submittal requirements may be modified by the Planning Director based on the nature of the proposal and the type of review necessary.

Expert Advice or Assistance

The City may obtain the advice of persons trained in the fields of architecture, landscape architecture, and urban planning to aid in its decision. It may be the policy of the City to have appointed to the Commission one or more persons trained in one or more of these fields as regular or ex-officio members thereof, provided that the private practice or business of such member does not create an incompatibility between the private practice or business and the rendition of objective advice to the Commission.

Minor Design Review Applications

The Planning Director may approve, conditionally approve, deny or refer to the Planning Commission an application for minor design review. Notice of hearing shall be given. Appropriate findings shall be made in writing. In the instance of a denial, the Planning Director shall state, in writing, grounds for disapproval.

Appeal of minor design review decisions may be made.

Major Design Review Applications

The Planning Commission may approve, conditionally approve, deny applications for major design review. Notice of hearings shall be given. Findings shall be made in the event an application is approved or conditionally approved. In the case of denial, the Commission shall state in writing the grounds for denial.

Appeal of major design review decisions may be made.

Required Findings

In approving or conditionally-approving an application for design review, the following findings must be made by either the Planning Director or the Planning Commission, as appropriate:

- A. The proposed development or use is consistent with all applicable policies and requirements of the General Plan and Zoning Ordinance.
- B. The proposed design of the development or use, and all appurtenant structures, is consistent with the policies set forth in the City of Healdsburg Design Review Manual, as may be amended from time to time.

- C. The proposed development or use is consistent with the purposes of the zoning district in which it is located.
- D. The proposed development or use is consistent with all other conditions imposed by the Planning Commission or City Council with respect to any matter related to the purpose of design review.

Effective Date and Term of Approval

The following shall govern date and term of design review approval.

- A. Approval of a design review application becomes effective 10 days from the date of action by the Planning Director or Planning Commission. The subject approval is granted for 12 months. If construction of the improvement(s) is not commenced within 12 months after the effective date, approvals will automatically expire. An extension of 12 months may be granted by the Planning Director upon written application by a responsible party prior to the expiration. Additional extensions of 12-month increments may be granted by the Planning Commission upon the written request of a responsible party prior to the expiration of the previous extension. In the case of all extensions, it must be found that there has been no change of circumstances that could prevent any of the required findings of approval to be made.
- B. The term of design review approval may be made to be concurrent with the term of an associated tentative subdivision map, use permit or variance at the discretion of the Planning Commission.

Appendix B

City Reference Documents

The following list of established plans, ordinances, policies and guidelines contain the criteria for development within the City of Healdsburg. Design proposals may be evaluated and approved based on compliance with these documents in addition to the guidelines stated herein:

General Plan

Zoning Ordinance

Guidelines and Standards for Historic Districts

Heritage Tree Ordinance